

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
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ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS	1A
PROJECT COMMITMENTS	1B
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DETAIL SHEETS	2G, 2G1-2G4
UTILITY NOTES AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	T1
TRAFFIC CONTROL PLANS	T2

YEAR	PROJECT NO.	SHEET NO.
2025	NH-I-40-6(187)	ROADWAY-SIGN1
	71I040-F8-011	
	71I040-M3-011	
	NH-I-40-6(188)	
	18I040-F8-009	

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE SHEET

\$\$\$\$SYTIME\$\$\$\$
\$\$\$\$DGN\$SPEC\$\$\$\$

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES X	NO

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	NH-I-40-6(187)	
STATE PROJ. NO.	711040-F8-011	
BRIDGE	711040-M3-011	
FED. AID PROJ. NO.	NH-I-40-6(188)	
STATE PROJ. NO.	181040-F8-009	

PUTNAM COUNTY

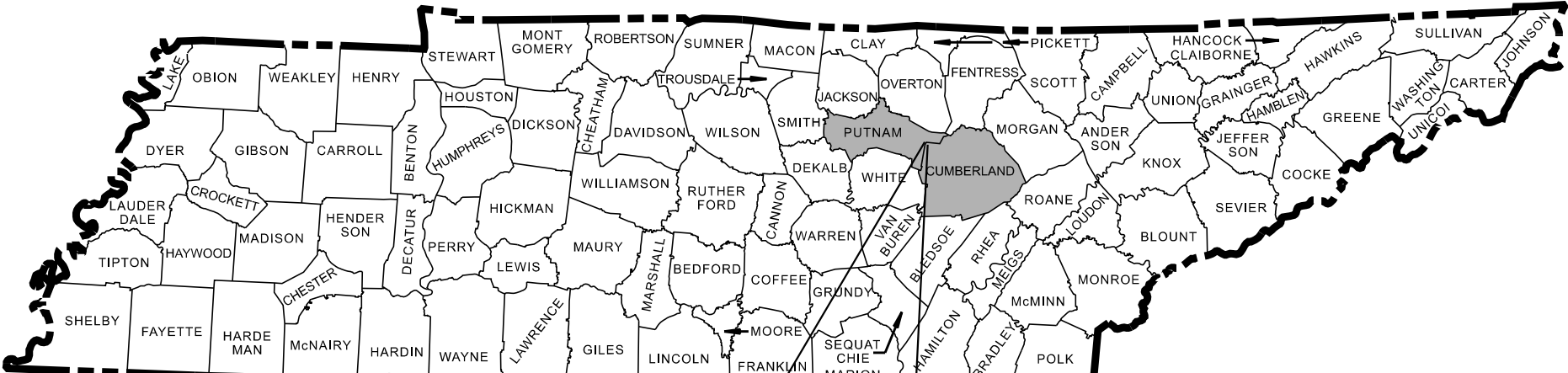
CUMBERLAND COUNTY

I-40
FROM WEST OF SR-24 (LM 0.00)
TO CUMBERLAND COUNTY LINE (LM 2.00)

I-40
FROM PUTNAM COUNTY LINE (LM 33.32)
TO NEAR PARKING AREA (LM 37.09)

RESURFACE
COLD PLANE, RESURFACE, INTELLIGENT COMPACTION,
NIGHTTIME WORK, BRIDGE REPAIR, AND PAVEMENT MARKINGS
STATE HIGHWAY NO. I-40 F.A.H.S. NO. N/A

RESURFACE
COLD PLANE, RESURFACE, INTELLIGENT COMPACTION,
NIGHTTIME WORK, AND PAVEMENT MARKINGS
STATE HIGHWAY NO. I-40 F.A.H.S. NO. N/A



PROJECT LOCATION

PROJECT LOCATION

BRIDGE ID. # 71100400053 , 71100400054 , 71100400055 , 71100400056 ,
71100400059 , 71100400057

BRIDGE ID. # 18100400001 , 18100400002 , 18100400003

BEGIN PROJECT NO. NH-I-40-6(187) RESURFACE
L.M. 33.32

0.50 MI EXCLUSION
PIN NO. 105727.01 (CUMBERLAND COUNTY)
BRIDGE REPLACEMENT L.M. 0.24
L.M. 36.59 - L.M. 37.09

711040-F8-011
711040-M3-011
END PROJECT NO. NH-I-40-6(187) RESURFACE
L.M. 37.09

181040-F8-009
BEGIN PROJECT NO. NH-I-40-6(188) RESURFACE
L.M. 0.00

1.29 MI EXCLUSION
PIN NO. 105727.01
BRIDGE REPLACEMENT L.M. 0.24
L.M. 0.00 - L.M. 1.29

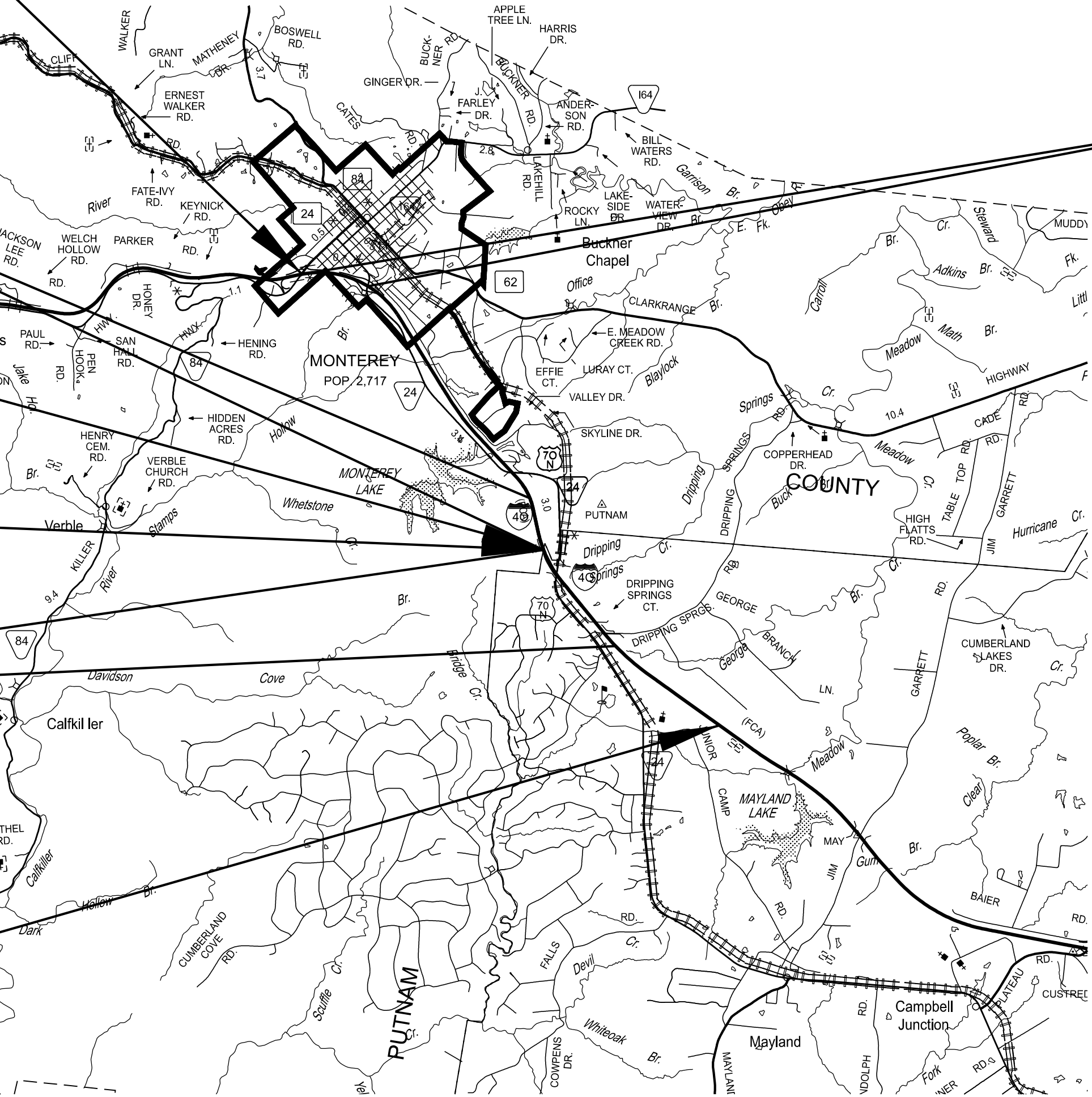
181040-F8-009
END PROJECT NO. NH-I-40-6(188) RESURFACE
L.M. 2.00

SPECIAL NOTES

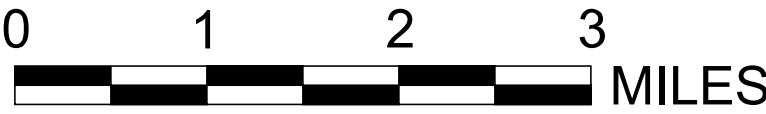
PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES
CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW
THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF
THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND
ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS
AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER : ANDREW ZAZZARA, P.E.
DESIGNED BY : ARCADIS
DESIGNER : ADAM GRAVITT
CHECKED BY : JAMES FRITZ BROGDON, P.E.
P.E. NO. 98023-4113-04 (DESIGN)
PIN NO. 132593.00; 132595.00



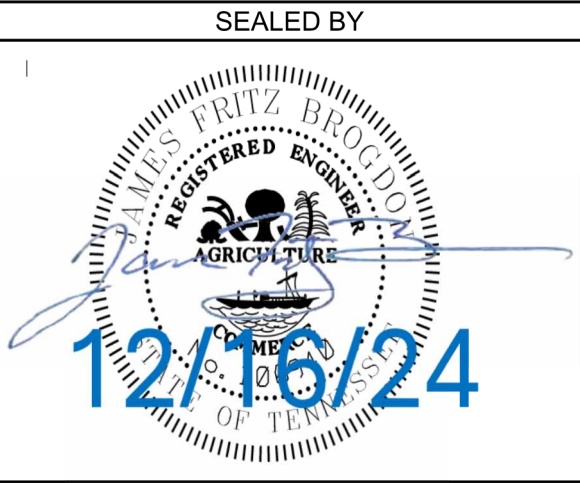
SCALE: 1"= 5280'



BRIDGE REPAIR PROJECT NO. 711010-M3-001
L.M. 33.46R & L.M. 34.12L

CUMBERLAND CO EXCLUSIONS	
STATION TO STATION	LENGTH (MI.)
0.00 - 1.29	1.29
TOTAL =	1.29

PUTNAM CO EXCLUSIONS	
STATION TO STATION	LENGTH (MI.)
36.59 - 37.09	0.50
TOTAL =	0.50



APPROVED: WILL REID, CHIEF ENGINEER

DATE:

APPROVED: HOWARD H. ELEY, COMMISSIONER

PUTNAM COUNTY TRAFFIC DATA	
ADT (2025)	39960
POSTED SPEED	70 MPH
CUMBERLAND COUNTY TRAFFIC DATA	
ADT (2025)	38400
POSTED SPEED	70 MPH

	I-40 PUTNAM	I-40 CUMBERLAND	TOTAL
PROJECT LENGTH	3.77 MILES	2.00 MILES	5.77 MILES
TOTAL LANE MILES RESURFACED	13.08 MILES	2.84 MILES	15.92 MILES

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED:

DIVISION ADMINISTRATOR DATE


ROADWAY INDEX

STANDARD ROADWAY DRAWINGS

SHEET NAME	SHEET NO.	DWG.	REV.	DESCRIPTION
SIGNATURE SHEET	ROADWAY-SIGN1	10-100.00 STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
TITLE SHEET	1			
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS.....	1A	RD-TP-1	10-01-24	STANDARD ROADWAY DRAWINGS TITLE SHEET
PROJECT COMMITMENTS	1B	RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
ESTIMATED ROADWAY QUANTITIES	2	RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B, 2B1	RD-L-1	02-20-20	STANDARD LEGEND
GENERAL NOTES.....	2C	RD-L-1A		STANDARD LEGEND
SPECIAL NOTES.....	2D	10-107.00 DESIGN - TRAFFIC CONTROL		
ENVIRONMENTAL NOTES.....	2E	T-M-1	06-28-19	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
TABULATED QUANTITIES	2F	T-M-5	03-01-23	MARKING DETAIL FOR FREEWAYS
DETAIL SHEETS	2G, 2G1-2G4	T-M-6	03-01-23	MARKING DETAIL FOR EXPRESSWAY AND FREEWAY INTERCHANGES
UTILITY NOTES AND UTILITY OWNERS.....	3	T-M-7	06-28-19	GORE MARKING DETAILS FOR EXPRESSWAY & FREEWAY INTERCHANGES
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	T1	T-M-8	06-28-19	MARKING DETAIL FOR EXPRESSWAYS & FREEWAYS
TRAFFIC CONTROL PLANS	T2	T-M-9	05-01-23	PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
NOTE: THE ALPHABETICAL LETTERS "I", "O" & "Q" ARE NOT USED IN NUMBERING OF SHEETS.		T-M-9A		PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
		T-M-9B		PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
		T-M-15	06-28-19	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR INTERSTATE AND ACCESS CONTROLLED ROUTES
		T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
		T-WZ-11	03-04-21	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
		T-WZ-12	03-04-21	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
		T-WZ-21	05-01-20	LANE CLOSURE WITH LEFT HAND MERGE AND LANE SHIFT
		T-WZ-60		FREEWAY RESURFACING SIGNING LAYOUT
		T-WZ-63	01-09-24	WORK ZONE IN THE VICINITY OF AN ENTRANCE RAMP
		T-WZ-64		WORK ZONE IN THE VICINITY OF AN EXIT RAMP
		T-WZ-FAB1		FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-L40-6(187)	1A
		71I040-F8-011	
		71I040-M3-011	
		NH-L40-6(188)	
		18I040-F8-009	

SEALED BY



12/16/24


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX
AND
STANDARD
ROADWAY
DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-40-6(187)	1B
		71I040-F8-011	
		71I040-M3-011	
		NH-I-40-6(188)	
		18I040-F8-009	

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	Environmental Division, Hazardous Materials	An Asbestos Containing Material (ACM) survey was performed on Bridge No. 71I00400053, I-40 WB over SR-24, LM 33.46 (71-I0040-33.46R). The bridge has 50 square feet of bearing pad material under the original beams at 70% chrysotile. Please see the reports for further details and photographs.	LM 33.46
EDHZ002	Environmental Division, Hazardous Materials	The State of Tennessee asbestos accreditation requirements (TDEC Rules Chapter 1200-01-20) mandates that ACM abatement work be performed by an accredited firm (contractor) using accredited abatement workers and supervisors. Abatement of this material shall be accomplished per SP202ACM Special Provision Regarding Removal of Asbestos-Containing Materials. ACM abatement should be completed prior to any demolition activities if possible. Prior to the demolition or rehabilitation of any structure (bridge or building), the contractor is required to submit the National Emission Standards for Hazardous Air Pollutants standard 10-day notice of demolition to the TDEC Division of Air Pollution Control (per TDOT Standard Specifications for Road and Bridge Construction (January 1, 2021) Sections 107.08.D and 202.03).	LM 33.46
EDHZ003	Environmental Division, Hazardous Materials	An Asbestos Containing Material (ACM) survey was conducted on Bridge No. 71I00400056, I-40 EB over SR-24, LM 34.12 (71-I0040-34.12L). No ACM was detected. No special accommodations for demolition and waste disposal are anticipated for this structure and the material can be deposited in a C&D landfill. Prior to the demolition or rehabilitation of any structure (bridge or building), the contractor is required to submit the National Emission Standards for Hazardous Air Pollutants standard 10-day notice of demolition to the TDEC Division of Air Pollution Control (per TDOT Standard Specifications for Road and Bridge Construction (January 1, 2021) Sections 107.08 D and 202.03).	LM 34.12

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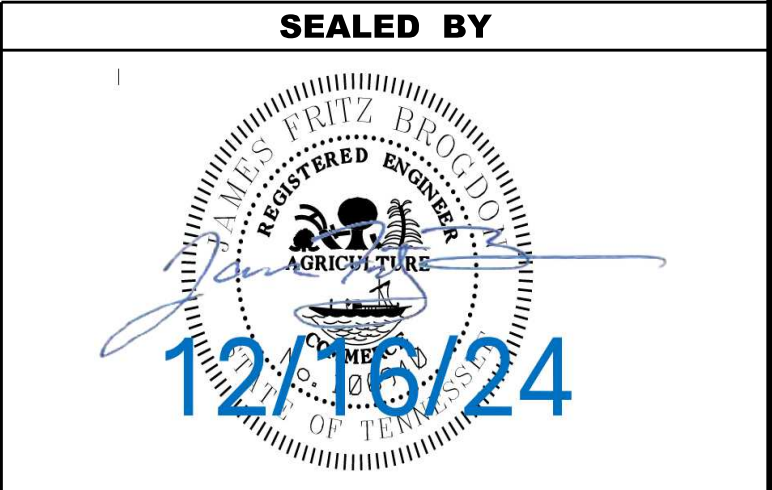


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROJECT
COMMITMENTS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-40-6(187)	2
		71I040-F8-011	
		71I040-M3-011	
		NH-I-40-6(188)	
		18I040-F8-009	

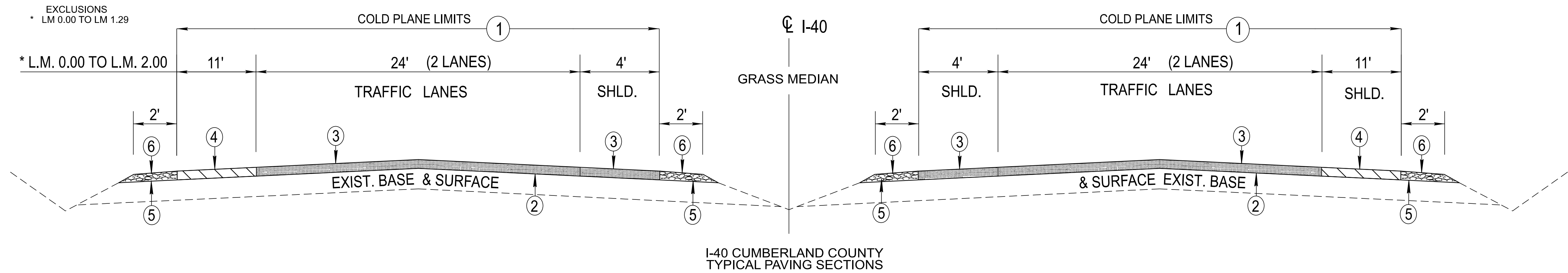
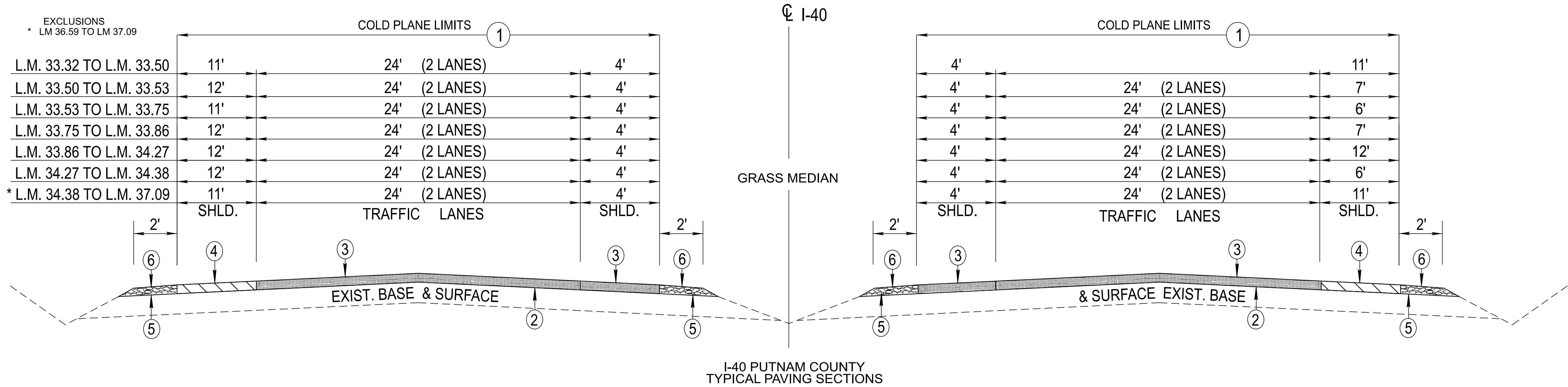
ESTIMATED ROADWAY QUANTITIES						FOOTNOTES	
ITEM NO.	DESCRIPTION	UNIT	QUANTITY 71I040-F8-011	QUANTITY 18I040-F8-009	TOTAL QUANTITY		
(1)	208-01.05 BROOMING & DEGRASSING SHOULDERS	L.M.	13.4	2.9	16.3	(1)	INCLUDES THE COST OF REMOVING DEBRIS AND SWEEPING SHOULDERS PRIOR TO WORK.
(2)	303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	2365	365	2730	(2)	PUTNAM CO. INCLUDES 188 TONS FOR EXIT #300 SHOULDERS, AND 182 TONS FOR EXIT #301 SHOULDERS. SEE SHEETS 2F, 2G, & 2G1 FOR DETAILS.
(3)	307-03.08 ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING B-M2	TON	1267		1267	(3)	TO BE USED FOR SPOT REPAIR AS DIRECTED BY THE TDOT PROJECT ENGINEER.
	402-01 BITUMINOUS MATERIAL FOR PRIME COAT (PC)	TON	22	5	27	(4)	TO BE APPLIED TO SHOULDER SCORING BEFORE PERMANENT EDGELINE MARKINGS ARE IN PLACE. SEE SHEET 2G FOR PLACEMENT DETAIL.
(4)	403-01.10 HIGH PERFORMANCE FOG SEALS	S.Y.	15723	3286	19009	(5)	PUTNAM CO. INCLUDES 8.1 TONS FOR EXIT #300, AND 8.9 TONS FOR EXIT #301. SEE SHEETS 2F, 2G, & 2G1 FOR DETAILS.
(5)	403-02.01 TRACKLESS TACK COAT	TON	93	11	104	(6)	PUTNAM CO. INCLUDES 169 TONS FOR EXIT #300 GORE AREAS AND 136 TONS FOR EXIT #301 GORE AREAS. SEE SHEET 2F FOR TABULATION AND SHEETS 2G & 2G1 FOR DETAILS.
	411-01.07 ACS MIX (PG64-22) GRADING E SHOULDER	TON	2820	607	3427	(7)	MILLED MATERIAL TO BECOME PROPERTY OF THE CONTRACTOR.
(6)	411-03.10 ACS MIX(PG76-22) GRADING D	TON	9275	1283	10558	(8)	PUTNAM CO INCLUDES 981 TONS FOR EXIT #300 RAMPS & 1079 TONS FOR EXIT #301 RAMPS, SEE SHEET NOS. 2F, 2G, & 2G1 FOR DETAILS.
(7)(8)	411-12.01 SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.	13.4	2.8	16.2	(9)	CUMBERLAND CO INCLUDES GUARDRAIL DELINEATION ENHANCEMENT. 549 WHITE & 99 YELLOW PUTNAM CO INCLUDES GUARDRAIL DELINEATION ENHANCEMENT. 1181 WHITE & 162 YELLOW SEE SHEET 2F FOR GUARDRAIL TABULATION BLOCK.
(9)	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	14380	2559	16939	(10)	INCLUDES ALL COSTS ASSOCIATED WITH NIGHTTIME WORK ZONE LIGHTING.
(10)	705-04.20 GUARDRAIL DELINEATION ENHANCEMENT	EACH	1343	648	1991	(11)	SEE SHEET 2F FOR TABULATION.
	712-01 TRAFFIC CONTROL	LS	0.82	0.18	1	(12)	SEE SHEET T2 FOR SUGGESTED PLACEMENT DETAILS.
	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	262	125	387	(13)	INCLUDES FOUR (4) MESSAGE BOARDS TO BE USED ON EXIT #300 & EXIT #301 ENTRANCE RAMPS TO I-40 AS DIRECTED BY THE TDOT PROJECT ENGINEER.
(11)	712-06 SIGNS (CONSTRUCTION)	S.F.	1136	399	1535	(14)	INCLUDES QUANTITY FOR CLEAR/RED SPM'S ALONG TRANSVERSE SHOULDER STRIPING, AND CLEAR/RED & YELLOW/RED SPM'S AT TIGHTER SPACING ALONG ROADWAY.
	712-08.03 ARROW BOARD (TYPE C)	EACH	2	2	4	(15)	TO BECOME PROPERTY OF THE CONTRACTOR. INCLUDES ALL COST ASSOCIATED WITH REMOVAL AND PROPER DISPOSAL.
	712-08.08 SPEED FEEDBACK SIGN ASSEMBLY	EACH	1	1	2	(16)	CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
(12)	712-08.09 DIGITAL SPEED LIMIT SIGN ASSEMBLY	EACH	6	3	9	(17)	FOR TEMPORARY PAVEMENT MARKINGS ON MILLED SURFACE AND INTERMEDIATE SURFACES.
	712-08.12 QUEUE PROTECTION TRUCK	DAY	21	21	42	(18)	ENHANCED FLATLINE THERMO PVMT MARKING SHALL BE APPLIED USING RIBBON METHOD AS DIRECTED BY THE TDOT PROJECT ENGINEER.
(13)	713-16.01 CHANGEABLE MESSAGE SIGN UNIT	EACH	5	1	6		
(14)	716-01.23 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	981	94	1075		
(15)	716-01.30 REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	981	94	1075		
(16)	716-02.05 PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	112		112		
(16)	716-02.07 PLASTIC PAVEMENT MARKING (24" BARRIER LINE)	L.F.	2678		2678		
(16)	716-02.24 PLASTIC PAVEMENT MARKING (12IN DWL)	L.F.	2410		2410		
(16)	716-04.06 PLASTIC PAVEMENT MARKING (WRONG WAY ARROW)	EACH	8		8		
(16)	716-04.07 PLASTIC PAVEMENT MARKING (EXIT ONLY ARROW)	EACH	4		4		
(16)	716-04.12 PLASTIC PAVEMENT MARKING (YIELD LINE)	S.F.	30		30		
(17)	716-05.05 PAINTED PAVEMENT MARKING (STOP LINE)	L.F.	112		112		
(17)	716-05.20 PAINTED PAVEMENT MARKING (6" LINE)	L.M.	21.4	3.2	24.6		
(18)	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	21.4	3.2	24.6		
(18)	716-12.03 ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE)	L.F.	7115		7115		
(18)	716-12.05 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN DOTTED LINE)	L.F.	992		992		
	717-01 MOBILIZATION	LS	0.82	0.18	1		



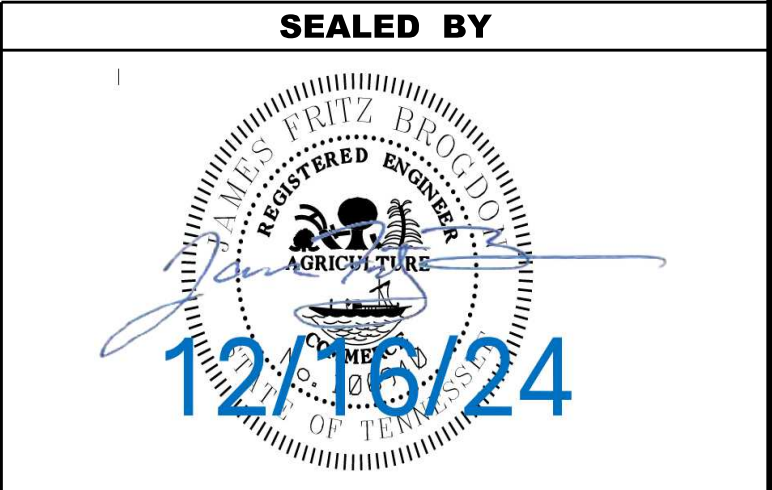
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-40-6(187)	2B
		711040-F8-011	
		711040-M3-011	
		NH-I-40-6(188)	
		181040-F8-009	



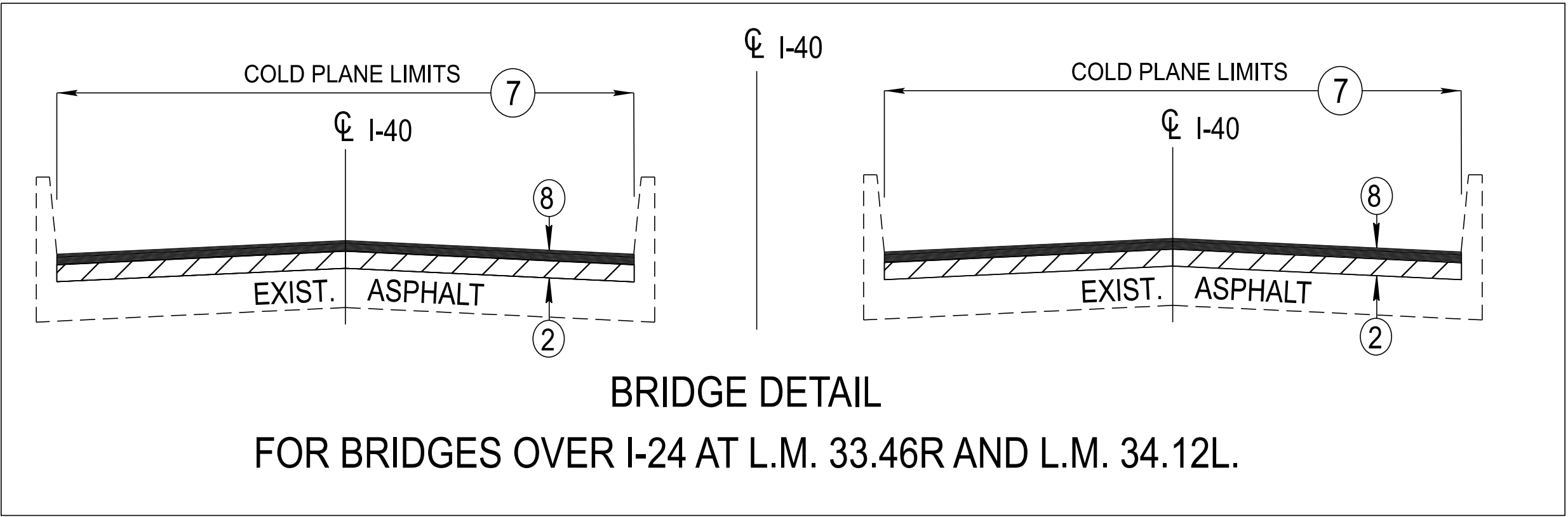
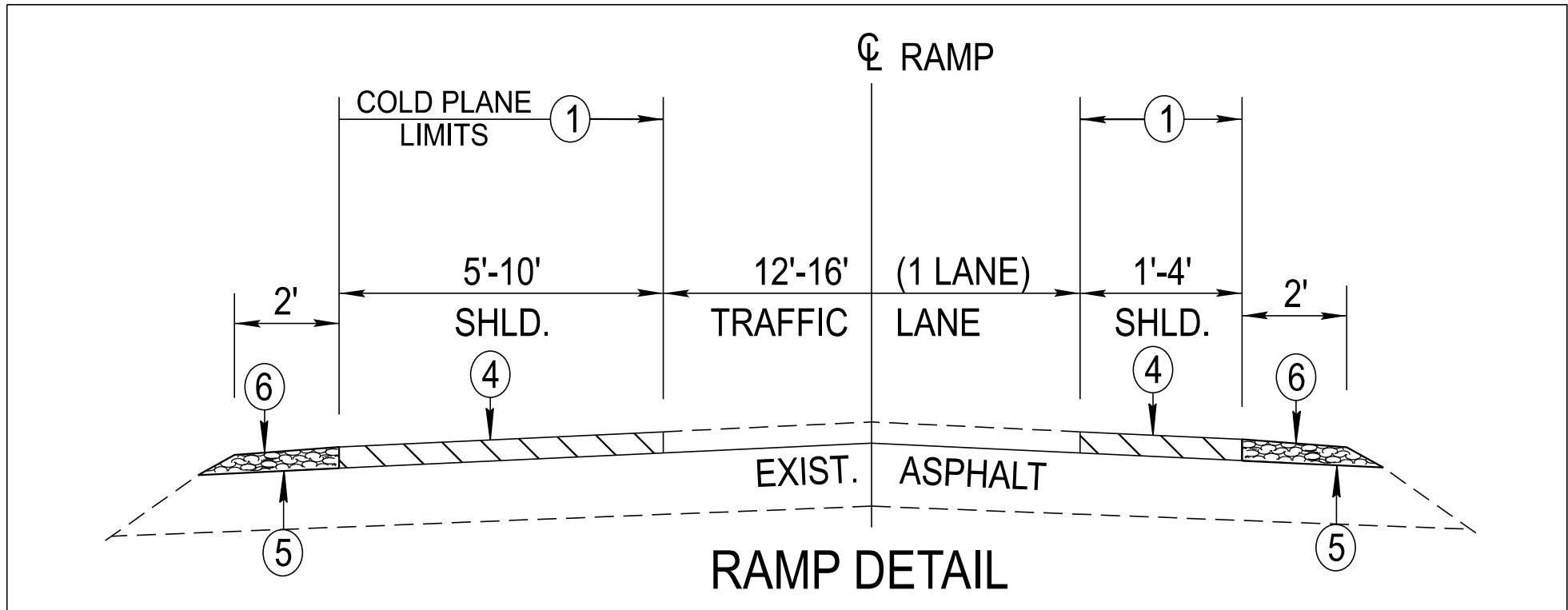
PROPOSED PAVEMENT SCHEDULE	
① COLD PLANING 1.50" THICK (APPROX. 157.50 LBS/SY) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT, TON	③ SURFACE MIX 1.25" THICK @ 132.5 LBS/SY ITEM 411-03.10 ACS MIX (PG76-22) GRADING "D", TON
② TRACKLESS TACK COAT (TC) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD. ITEM 403-02.01 TRACKLESS TACK COAT (TC), TON	④ PAVED SHOULDERS 1.25" THICK @ 132.5 LBS/SY ITEM 411-01.07 ACS MIX (PG64-22) GRADING "E" SHOULDER, TON
⑤ MINERAL AGGREGATE BASE 2" THICK @ 2.03 TON/CY ITEM 303-01 MINERAL AGGREGATE, TYPE "A" BASE, GRADING "D", TON	⑥ SHOULDER STONE STABILIZATION 401-01 BITUMINOUS MATERIAL FOR PRIME COAT (PC), TON ***PRIME COAT WIDTH SHALL BE A MINIMUM OF 3 FEET FROM EDGE OF PAVED SHOULDER AND AROUND GUARDRAIL POST AS DIRECTED BY TDOT ENGINEER



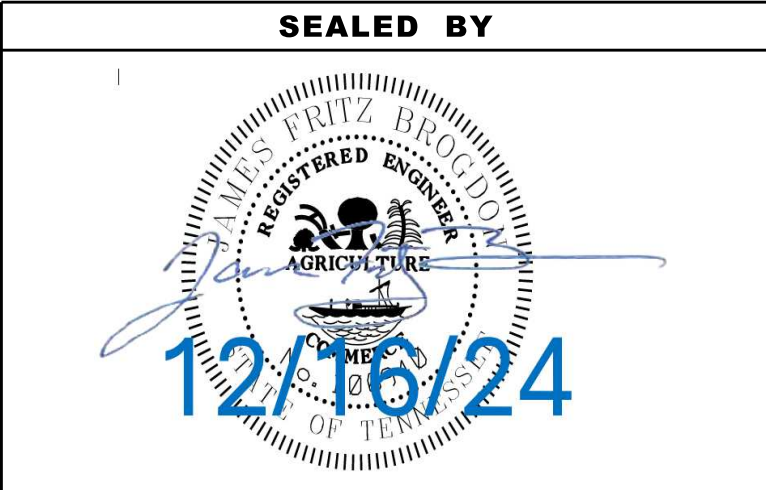
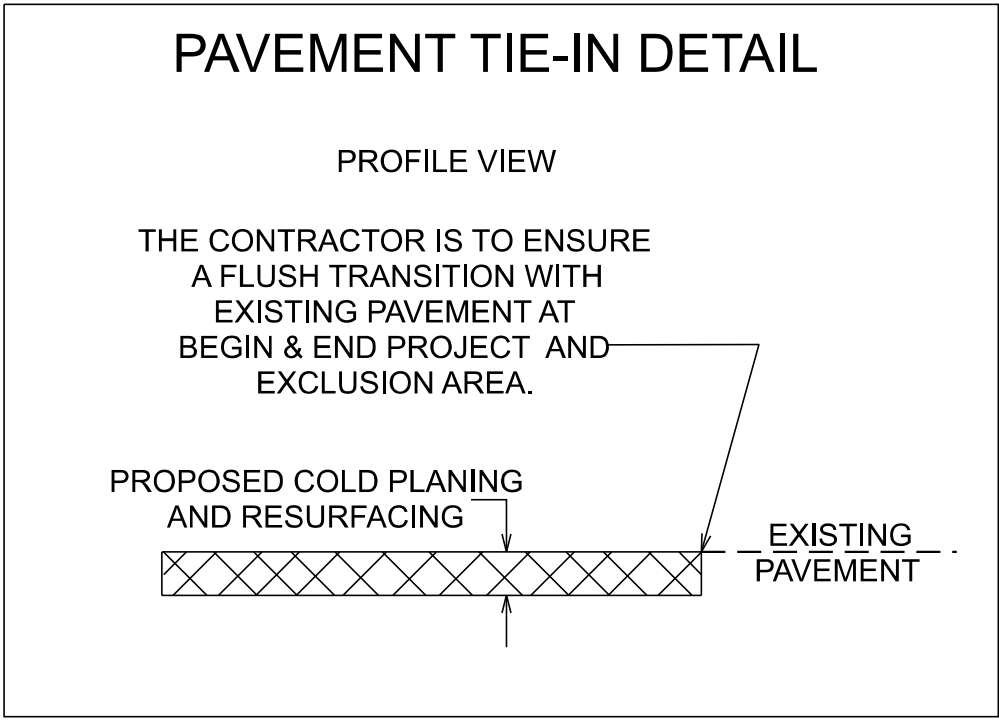
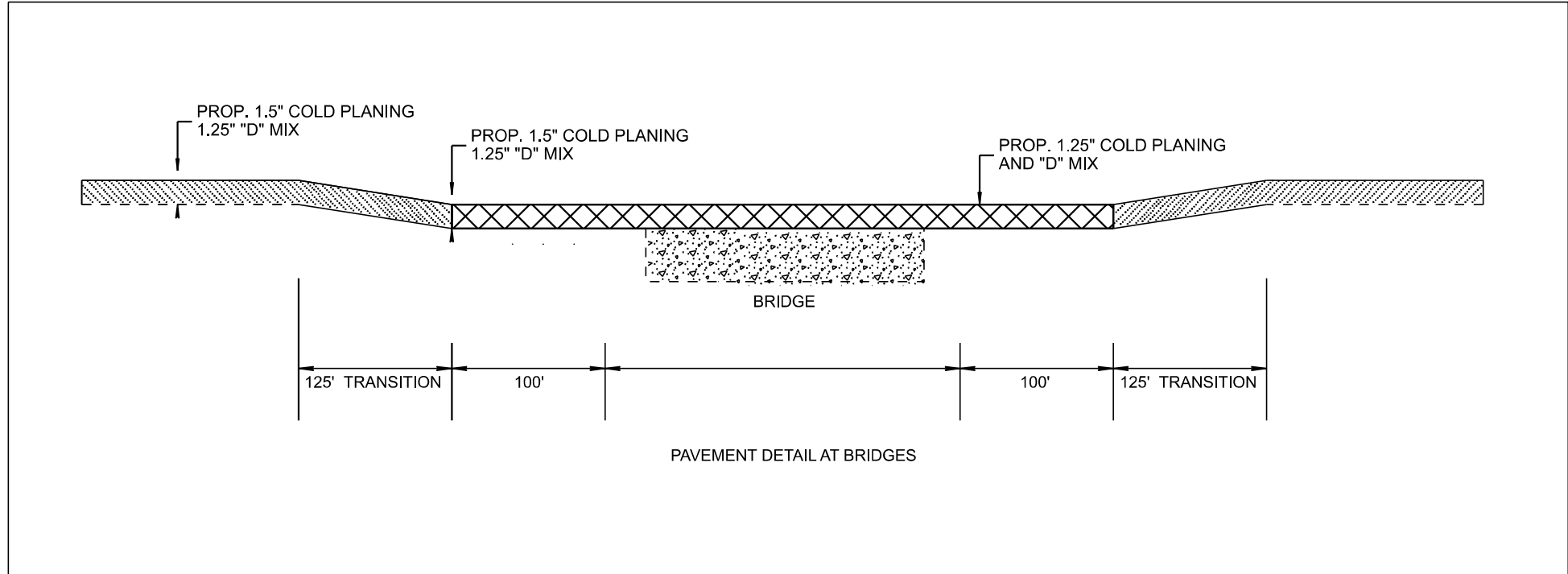
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-40-6(187)	2B1
		71I040-F8-011	
		71I040-M3-011	
		NH-I-40-6(188)	
		18I040-F8-009	



PROPOSED PAVEMENT SCHEDULE	
① COLD PLANING 1.50" THICK (APPROX. 157.50 LBS/SY) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT, TON	③ SURFACE MIX 1.25" THICK @ 132.5 LBS/SY ITEM 411-03.10 ACS MIX (PG76-22) GRADING "D", TON
② TRACKLESS TACK COAT (TC) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD. ITEM 403-02.01 TRACKLESS TACK COAT (TC), TON	④ PAVED SHOULDERS 1.25" THICK @ 132.5 LBS/SY ITEM 411-01.07 ACS MIX (PG64-22) GRADING "E" SHOULDER, TON
⑤ MINERAL AGGREGATE BASE 2" THICK @ 2.03 TON/CY ITEM 303-01 MINERAL AGGREGATE, TYPE "A" BASE, GRADING "D", TON	⑥ SHOULDER STONE STABILIZATION 401-01 BITUMINOUS MATERIAL FOR PRIME COAT (PC), TON ***PRIME COAT WIDTH SHALL BE A MINIMUM OF 3 FEET FROM EDGE OF PAVED SHOULDER AND AROUND GUARDRAIL POST AS DIRECTED BY TDOT ENGINEER
⑦ COLD PLANING 2.75" THICK (APPROX. 288.75 LBS/SY) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT, TON	⑧ SURFACE MIX 2.75" THICK @ 291.5 LBS/SY ITEM 411-03.10 ACS MIX (PG76-22) GRADING "D", TON



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

MISCELLANEOUS

- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6" LINE), L.M.

FINAL PAVEMENT MARKING

- (7) IN AREAS WITH OPEN-GRADED FRICTION COURSE PAVEMENT, THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

a. SHOULDERS SHALL BE BROOMED AND DE-GRASSED IN A MANNER WHICH PERMITS PROPER DRAINAGE OF PAVEMENT STRUCTURE. MATERIAL SHALL BE PICKED UP AND REMOVED. THIS WILL BE PAID FOR UNDER ITEM NO. 208-01.05.

b. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (9) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (20) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

TRAFFIC CONTROL DIRECTIONAL SIGNING

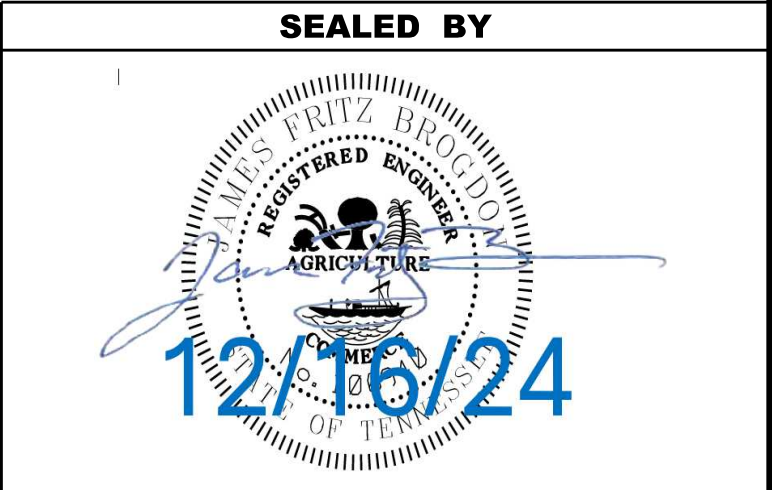
- (1) ON ALL ACCESS CONTROLLED AND INTERSTATE RECONSTRUCTION AND NEW CONSTRUCTION PROJECTS, THE CONTRACTOR SHALL UTILIZE ALL EXISTING DIRECTIONAL SIGNING FOR AS LONG AS POSSIBLE. THESE EXISTING SIGNS CAN BE MOVED USING TEMPORARY SUPPORTS AS NEEDED. AS SOON AS THESE EXISTING DIRECTIONAL SIGNS COME DOWN PERMANENTLY, THE CONTRACTOR SHALL HAVE UP AT LEAST ONE NEW TEMPORARY "ADVANCE GUIDE SIGN" AND ONE NEW TEMPORARY "EXIT DIRECTIONAL SIGN" AT ALL EXIT RAMPS. THESE SIGNS ARE TO BE MAINTAINED WITHIN CLEAR VIEW OF THE PUBLIC ON THE RIGHT SIDE OF THE HIGHWAY AND SHALL BE REPLACED IF DAMAGED, DURING ALL PHASES OF CONSTRUCTION, AS DIRECTED BY THE ENGINEER.
- (2) THE SIZE OF THESE NEW TEMPORARY SIGNS WILL BE DETERMINED BY THE MESSAGE. THE MESSAGE SHALL BE THE SAME AS THE EXISTING SIGN THAT THESE NEW TEMPORARY SIGNS WILL BE REPLACING. THE LETTER SIZE SHALL BE A MINIMUM OF 8 INCH, "D" UPPER CASE LETTER. THE DIRECTIONAL ARROW WILL BE A "B" ARROW AT A 45 DEGREE ANGLE (SAME ANGLE AS THE EXISTING ARROW). THE MATERIAL SHALL BE 0.100 INCH SHEET ALUMINUM; THE COLOR SHALL BE A REFLECTIVE GREEN BACKGROUND WITH REFLECTIVE WHITE COPY.
- (3) ALL WORK AND MATERIAL TO MAKE THESE NEW TEMPORARY DIRECTIONAL SIGNS ALONG WITH ADEQUATE SUPPORTS AND TO MOVE THEM AS NEEDED DURING EACH PHASE OF CONSTRUCTION WILL BE PAID FOR UNDER ITEM NO. 712-06, AS DIRECTED BY THE ENGINEER.
- (4) SOME OF THESE DIRECTIONAL SIGNS WILL NEED AN INTERSTATE, U.S., OR A STATE HIGHWAY SHIELD, A CARDINAL DIRECTION, AND A DIRECTION ARROW TO ACCOMPANY THE DIRECTIONAL SIGN. THESE SIGNS SHALL BE MOUNTED BELOW THE DIRECTIONAL SIGN.
- (5) ALL EXISTING "EMERGENCY REFERENCE MARKERS" AND "HOSPITAL SIGNS" SHALL BE MAINTAINED WITHIN FULL VIEW OF THE MOTORING PUBLIC THROUGHOUT ALL PHASES OF CONSTRUCTION. ALL WORK IN MOVING AND TEMPORARY SUPPORTS SHALL BE PAID FOR UNDER ITEM NO. 712-06.
- (6) WHEN "LOGO" SIGNS ARE ON ACCESS CONTROLLED AND INTERSTATE RECONSTRUCTION AND NEW CONSTRUCTION PROJECTS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THESE SIGNS IN FULL VIEW TO THE MOTORING PUBLIC DURING ALL PHASES OF CONSTRUCTION. THE CONTRACTOR SHALL BE HELD RESPONSIBLE TO THE DEPARTMENT FOR THE REIMBURSEMENT OF THE SIGN FACE IF IT IS DAMAGED. ALL WORK IN MOVING THESE "LOGO" SIGNS AND THE TEMPORARY SUPPORTS ARE TO BE PAID FOR UNDER ITEM NO. 712-06, AS DIRECTED BY THE ENGINEER. THE SUPPORTS FOR THE FINAL LOCATION OF THESE SIGNS WILL BE PAID FOR UNDER OTHER ITEMS OF CONSTRUCTION.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-40-6(187)	2C
		711040-F8-011	
		711040-M3-011	
		NH-I-40-6(188)	
		181040-F8-009	



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL
NOTES

SPECIAL NOTES

TRAFFIC CONTROL

- (1)

THE CONTRACTOR SHALL MAKE PROVISIONS TO SAFELY CONTROL TRAFFIC INGRESS AT ALL ROADWAY ENTRANCES WITHIN THE FLAGGED WORK ZONE TO THE SATISFACTION OF THE TDOT PROJECT ENGINEER. ALL COST ASSOCIATED WITH THE CONTROLS WILL BE INCLUDED IN THE PRICE BID FOR TRAFFIC CONTROL.
- (2)

EXISTING CONSTRUCTION, REGULATORY AND WARNING SIGNS WHICH CONFLICT WITH THE CONSTRUCTION SIGNING SHALL BE REMOVED DURING CONSTRUCTION AND REINSTALLED AS DIRECTED BY THE TDOT PROJECT ENGINEER. ALL COSTS TO BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-01, TRAFFIC CONTROL, PER LUMP SUM.
- (3)

THE CONTRACTOR SHALL GIVE THE TDOT PROJECT ENGINEER A MINIMUM OF SEVEN (7) DAYS NOTICE PRIOR TO STARTING WORK SO THAT SUFFICIENT NOTICE CAN BE PREPARED AND DISTRIBUTED TO THE MEDIA.
- (4)

IMMEDIATELY UPON COMPLETION OF EACH CONSTRUCTION PHASE, ALL TRAFFIC CONTROL ITEMS THAT ARE NOT NECESSARY FOR THE SUCCEEDING PHASE SHALL BE REMOVED, COVERED, OR TURNED TO FACE AWAY FROM THE TRAFFIC.
- (5)

THE CONTRACTOR SHALL PROVIDE FLAGGER AT EACH SIDEROAD WITHIN THE PAVING OPERATION.
- (6)

A MINIMUM OF TWO (2) TYPE "C" ARROW BOARDS WILL BE REQUIRED. IF THE CONTRACTOR SCHEDULES HIS OPERATIONS SUCH THAT MORE THAN TWO (2) ARROW BOARDS ARE REQUIRED. THE COST OF ALL ADDITIONAL ARROW BOARDS WILL BE AT THE CONTRACTOR'S EXPENSE.
- (7)

CHANGEABLE MESSAGE SIGNS UNITS (ITEM NO. 713-16.01) WILL BE REQUIRED ON THIS PROJECT. THE SIGNS WILL BE USED FOR ADVANCED WARNING OR AS DIRECTED BY THE ENGINEER.
- (8)

MESSAGE BOARDS SHALL BE DISPLAYED A MINIMUM OF SEVEN (7) DAYS PRIOR TO STARTING WORK TO INFORM PUBLIC OF UPCOMING CONSTRUCTION PROJECT.
- (9)

PRIOR TO DISPLAYING MESSAGES ON CHANGEABLE MESSAGE BOARDS, ALL MESSAGES SHALL BE APPROVED BY THE TDOT PROJECT ENGINEER AND REGION 2 TRAFFIC ENGINEER.
- (10)

INFORMATION ON CHANGEABLE MESSAGE BOARDS SHALL BE UPDATED TO REFLECT CURRENT CONSTRUCTION CONDITIONS ACTIVITIES AT ALL TIMES.

NIGHT WORK

- (1)

ALL COST ASSOCIATED WITH NIGHT WORK ZONE LIGHTING SHALL BE INCLUDED IN ITEM 712-01.
- (2)

THE CONTRACTOR SHALL COMPLY WITH SECTION 712.04 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING NIGHT WORK LIGHTING.
- (3)

LIGHTS FOR NIGHT WORK TO BE APPROVED BY THE TDOT ENGINEER.
- (4)

NIGHT WORK SHALL BE DONE BETWEEN THE HOURS OF 7:00 PM AND 6:00 AM. ALL WORK TO BE DONE AT NIGHT SHALL BE DURING THESE HOURS. THIS INCLUDES CONSTRUCTION, MOBILIZATION OF EQUIPMENT AND PLACEMENT AND REMOVAL OF TRAFFIC CONTROL DEVICES.
- (5)

ALL CONSTRUCTION SIGNS THAT ARE TO REMAIN IN PLACE DURING CONSTRUCTION MAY BE INSTALLED DURING DAYTIME HOURS AS DIRECTED BY THE TDOT PROJECT SUPERVISOR.

END OF TRAFFIC QUEUE PROTECTION

- (1)

WHEN INTERSTATE LANE CLOSURES ARE PERFORMED, THE CONTRACTOR SHALL PROVIDE A MINIMUM OF TWO (2) TRUCK MOUNTED MOBILE CHANGEABLE MESSAGE SIGNS WITH TRUCK MOUNTED ATTENUATORS FOR EACH TRAVELING DIRECTION IN WHICH LANES ARE CLOSED, TO PROTECT THE END OF THE TRAFFIC QUEUE.

A.

AT LEAST ONE UNIT SHALL BE POSITIONED ON THE SHOULDER UPSTREAM FROM THE END OF THE TRAFFIC QUEUE AT ALL TIMES, BUT NO FURTHER THAN 1/2 MILE FROM THE QUEUE.

B.

THE CHANGEABLE MESSAGE SIGN SHALL DISPLAY TWO PHASES: "STOPPED TRAFFIC AHEAD" AND "BE PREPARED TO STOP".

- C.

IN THE EVENT THAT THE TRAFFIC QUEUE EXTENDS UPSTREAM BEYOND THE UNIT, A SECOND UNIT SHALL MOBILIZE IN ACCORDANCE WITH [A.]. THE DOWNSTREAM UNIT MAY DEPART WHEN THE SECOND UNIT HAS MOVED INTO POSITION.
- D.

TRUCK MOUNTED CHANGEABLE MESSAGE SIGNS AND ATTENUATORS SHALL BE IN ACCORDANCE WITH THE NCHRP REPORT 350, MASH TL-3, MUTCD, AND MOUNTED PER THE MANUFACTURER'S RECOMMENDATIONS. TRUCK MOUNTED CHANGEABLE MESSAGE SIGNS SHALL BE LISTED ON QPL LIST 30, SECTION B, AND TRUCK MOUNTED ATTENUATORS SHALL BE LISTED ON QPL LIST 34, SECTION D.
- E.

MOBILE CHANGEABLE MESSAGE SIGN UNITS WITH MOUNTED ATTENUATOR WILL BE PAID FOR AS ITEM NO. 712-08.12 QUEUE PROTECTION TRUCK, DAY. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID.

LANE CLOSURES

- (1)

THE RESTRICTION OF TRAFFIC TO ONE LANE SHALL NOT EXCEED ONE DAY'S PAVING OPERATION.
- (2)

THE CONTRACTOR SHALL KEEP ALL TRAFFIC LANES OPEN TO TRAFFIC DURING NON-WORKING HOURS AND/OR NON-WORK DAYS.
- (3)

DAYTIME LANE CLOSURES WILL NOT BE ALLOWED UNLESS DIRECTED BY THE TDOT PROJECT ENGINEER.
- (4)

THE CONTRACTOR SHALL NOT BE ALLOWED TO INTERRUPT TRAFFIC FLOW AND SHALL MAINTAIN ALL LANES OF TRAFFIC IN EACH DIRECTION ON THE FOLLOWING DAYS:

A.

OFFICIAL STATE HOLIDAYS.

B.

FRIDAY AT 6:00 AM UNTIL TUESDAY AT 7:00 PM. IF A STATE HOLIDAY OCCURS OR IS OBSERVED ON MONDAY.

C.

THURSDAY AT 6:00 AM UNTIL SUNDAY AT 7:00PM. IF A STATE HOLIDAY OCCURS OR IS OBSERVED ON FRIDAY.

D.

DURING LOCAL FESTIVALS, GAMES OR EVENTS THAT COULD BE IMPEDED BY THE PAVING OPERATIONS WHERE AND AS DIRECTED BY THE TDOT PROJECT ENGINEER.
- PAVING
- (1)

CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

•

BROOMING & DEGRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OR ROADWAY AND SHOULDERS AS DIRECTED BY THE TDOT PROJECT ENGINEER.

•

ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED.

•

ALL COSTS ASSOCIATED WITH CLIPPING, BROOMING AND DE-GRASSING SHALL BE PAID FOR UNDER ITEM NO. 208-01.05

(2)

INTELLIGENT COMPACTION SHALL BE USED.

(3)

ANY QUANTITY REMAINING ON ITEMS COMPLETED PRIOR TO THE PAVING OPERATION WILL NOT BE CONVERTED TO ADDITIONAL ASPHALT FOR THE ROADWAY.

(4)

THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK.

•

REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- PAVEMENT MARKING
- (1)

THE CONTRACTOR IS RESPONSIBLE FOR THE LAYOUT OF ALL PAVEMENT MARKING. ANY CHANGE FROM THE EXISTING PAVEMENT MARKINGS MUST BE APPROVED BY THE REGION2 TRAFFIC MANAGER.

(2)

EXISTING PAVEMENT MARKING IN THE SIDE ROAD RADII SHALL BE RE-TRACED AS DIRECTED BY THE TDOT PROJECT ENGINEER DURING FINAL STRIPING.
- COLD PLANING
- (1)

THE CONTRACTOR WILL BE REQUIRED TO PAVE THE COLD PLANED PAVEMENT WITHIN A PERIOD OF 96 HOURS.

(2)

PROVIDED THAT THE CONTRACTOR USES A COLD PLANING MACHINE WITH THE MILLING DRUM SPECIFICATIONS OF CLOSELY SPACED FINE TOOTH (5/16 INCHES OR SMALLER) MILLING DRUM, MOVING FORWARD AT MODERATE SPEED (50 TO 65 FPM.) AND THE UNDER SURFACE REMAINS INTACT (HAVING NO LOOSE MATERIAL OR GOUGES,) THEN THE CONTRACTOR MAY PERMISSION FROM THE REGION 2 PROJECT ENGINEER TO MILL ALL SURFACES BEFORE COVERING WITH BITUMINOUS MATERIAL.

(3)

IF MILLED SURFACE BEGINS TO DETERIORATE, PAVING TO COVER UP DETERIORATING MILLED SURFACES SHOULD OCCUR AS DIRECTED BY THE TDOT PROJECT ENGINEER DURING THE NEXT WORKING DAY. IF SEVERE DISTRESS OCCURS, IMMEDIATE RESPONSE WILL BE REQUIRED.

(4)

IF SUBSURFACE PAVEMENT FAILURE IS EXPOSED AFTER MILLING, REMOVE AREA OF FAILURE TO SOUND PAVEMENT AND REPLACE WITH "BM2" MIX PRIOR TO PLACING PROPOSED OVERLAY.

(5)

AT THE COMPLETION OF COLD PLANING OPERATION, ANY DEPRESSIONS WILL BE CLEANED AND BROUGHT TO PROPER GRADE WITH "BM2" MIX MATERIAL. THE COST OF THE CLEAN UP WILL BE INCLUDED IN THE UNIT PRICE FOR "BM2" MIX MATERIAL.

(6)

THE CONTRACTOR SHOULD TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT NEAR EXISTING MANHOLES AS NOT TO DAMAGE MANHOLE OR CONCRETE RING AROUND EXISTING MANHOLE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE TDOT PROJECT ENGINEER AT NO ADDITIONAL COST.

(7)

EXISTING CONCRETE POURED AROUND CATCH BASINS AT THE GUTTER SHALL NOT BE DAMAGED. THIS CONCRETE MUST REMAIN IN PLACE WITH ASPHALT TYING TO THE EXISTING CONCRETE. ANY DAMAGE WILL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE DEPARTMENT.

(8)

ADJUSTMENT OF EXISTING MANHOLE INCLUDES LOWERING BEFORE MILLING OPERATION BEGINS AND RAISING BACK UP AFTER FINAL PAVING.

(9)

IF SUBSURFACE PAVEMENT FAILURE IS EXPOSED AFTER MILLING, REMOVE AREA OF FAILURE TO SOUND PAVEMENT AND REPLACE WITH "CM" MIX PRIOR TO PLACING PROPOSED OVERLAY.
- FOG SEALING SHOULDERS
- (1)

THE CONTRACTOR SHALL USE HIGH PERFORMANCE (HP) FOG SEAL FROM QUALIFIED PRODUCTS LIST QPL40.001.

(2)

THE SCORING OF SHOULDERS SHALL BE COMPLETED PRIOR TO APPLYING HP FOG SEAL TO PREVENT WATER INTRUSION.

(3)

FINAL EDGE LINE STRIPING SHALL NOT BE APPLIED UNTIL HP FOG SEAL APPLICATION HAS BEEN COMPLETED.
- | TYPE | YEAR | PROJECT NO. | SHEET NO. |
|---------|------|---------------|-----------|
| RESURF. | 2025 | NH-L40-6(187) | 2D |
| | | 711040-F8-011 | |
| | | 711040-M3-011 | |
| | | NH-L40-6(188) | |
| | | 181040-F8-009 | |
- SEALED BY
- STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL
NOTES
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ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4)

THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9)

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11)

SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12)

IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15)

IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

SCOPE OF WORK

- (6)

COLD PLANE, RESURFACE, INTELLIGENT COMPACTION, NIGHTTIME WORK, BRIDGE REPAIR, AND PAVEMENT MARKINGS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1)

IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

- (6)

EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.

- (8)

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29)

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30)

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31)

CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32)

WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33)

IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34)

ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35)

WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36)

ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37)

ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38)

OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED
- LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39)

DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.

(40)

WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.
- SPILL PREVENTION, MANAGEMENT & NOTIFICATION
- (44)

ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.

(45)

FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.

(46)

APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.

(47)

ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.

(48)

THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.

(49)

IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.

(50)

FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.

(51)


IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

(52)

WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR1000000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.

(53)

CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.
- | TYPE | YEAR | PROJECT NO. | SHEET NO. |
|---------|------|----------------|-----------|
| RESURF. | 2025 | NH-I-40-6(187) | 2E |
| | | 71I040-F8-011 | |
| | | 71I040-M3-011 | |
| | | NH-I-40-6(188) | |
| | | 18I040-F8-009 | |
- SEALED BY


- STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL
NOTES
- 12/12/2024 10:32:06 AM L:\SURVDESIGN\PROJECTS_ORD\RESURFACING\IGNPIN-SHT-ENVIRONMENTALNOTES.DGN

PUTNAM CO. I-40 EXITS #300, #301 ENTRANCE / EXIT RAMPS									
LOCATION		QUANTITY				QUANTITY			
EXIT	RAMP	STONE	COLD PLANE	ACS MIX GRADING "D" GORE AREAS	TRACKLESS TACK COAT	PAINTED PVMT MARKING (6" LINE)	ENHANCED FLATLINE THERMO (6" LINE)	ENHANCED FLATLINE THERMO (8" BARRIER LINE)	ENHANCED FLATLINE THERMO (6" DOTTED LINE)
		303-01 TON	415-01.01 TON	411-03.10 TON	403-02.01 TON	716-05.20 L.M.	716-12.02 L.M.	716-12.03 L.F.	716-12.05 L.F.
300	SW EXIT (40 EB TO 70)	53	188	16	1.54	0.58	0.58	1055	81
	SE ENT (70 TO 40 EB)	53	303	47	2.49	0.59	0.59	1606	
	NE EXIT (40 WB TO 70)	34	242	45	1.99	0.37	0.37	1530	
	NW EXIT (70 TO 40 EB)	48	248	61	2.04	0.54	0.54	913	80
301	SW EXIT (40 EB TO 70)	73	469	51	3.85	0.83	0.83	1370	475
	SE ENT (70 TO 40 EB)	38	191	57	1.57	0.41	0.41	82	306
	NE EXIT (40 WB TO 70)	33	170	8	1.40	0.36	0.36	160	50
	NW EXIT (70 TO 40 EB)	38	249	20	2.05	0.42	0.42	399	
TOTALS		370	2060	305	16.93	4.10	4.10	7115	992

UTILITY ADJUSTMENTS (RESURFACING)

THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT

STORM DRAIN ADJUSTMENTS (RESURFACING)

THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I40-6(187)	2F
		71I040-F8-011	
		71I040-M3-011	
		NH-I40-6(188)	
		18I040-F8-009	

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)								
					PUTNAM CO		CUMBERLAND CO	
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES			TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
E5-2A	EXIT CLOSED	48"	x	36"	12	24		
G20-1	ROAD WORK NEXT 4 MILES	48"	x	24"	8	16	2	16
G20-2	END ROAD WORK	48"	x	24"	8	32	2	16
R1-2	YIELD	48"	x	24"	8	8	2	16
W4-2L	MERGE LEFT SYMBOL	48"	x	48"	16	64	2	32
W8-11	UNEVEN LANES	48"	x	48"	16	256	2	32
W8-15	GROOVED PAVEMENT	48"	x	48"	16	256	2	32
W8-15P	MOTORCYCLE (PLAQUE)	30"	x	24"	5	80	2	10
W8-17	SHOULDER DROP-OFF SYMBOL	48"	x	48"	16	128	1	16
18-17P	SHOULDER DROP-OFF PLAQUE	30"	x	24"	5	40	1	5
W20-1	ROAD WORK AHEAD	48"	x	48"	16	32		
W20-1	ROAD WORK 1 MILE	48"	x	48"	16	32	2	32
W20-1	ROAD WORK 1/2 MILE	48"	x	48"	16	32	2	32
W20-1	ROAD WORK 1000 FT	48"	x	48"	16	32	2	32
W20-5R	RIGHT LANE CLOSED 1 MILE	48"	x	48"	16	32	2	32
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48"	x	48"	16	32	2	32
W20-5R	RIGHT LANE CLOSED 1500 FT	48"	x	48"	16	32	2	32
W21-2	FRESH OIL	48"	x	48"	16	32	2	32
					PUTNAM TOTAL	1136	CUMBERLAND TOTAL	399

PUTNAM CO. BRIDGE DECK RECOMMENDATIONS (RESURFACING)				
BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK RECOMMENDATIONS
71I00400053	33.46R	SR-24	277'	REMOVE ASPHALT - PROVIDE DECK REPAIR ITEMS - SEAL WITH MEMBRANE COLD PLANE 3.5" OF THE EXISTING ASPHALT AND REPLACE WITH 3.25" OF NEW ASPHALT.
71I00400054	33.46L	SR-24	257'	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT.
71I00400055	34.12R	SR-24 E. STRATTON AVE	147'	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT.
71I00400056	34.12L	SR-24 E. STRATTON AVE	147'	REMOVE ASPHALT - PROVIDE DECK REPAIR ITEMS - SEAL WITH MEMBRANE COLD PLANE 3.75" OF THE EXISTING ASPHALT AND REPLACE WITH 3.25" OF NEW ASPHALT.
71I00400059	34.160	STAMPS HOLLOW BRANCH	N/A	PAVE WITH PLANS/TREATMENT TYPE
71I00400057	36.100	SR-24 CROSSVILLE HWY	N/A	MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT

CUMBERLAND CO. PROPOSED GUARDRAIL (RESURFACING)			
QTY (EACH)		ITEM NO. 705-04.20	COMMENTS
WHITE	YELLOW	GUARDRAIL DELINEATION ENHANCEMENT (B+DIR) EACH	DELINEATORS SHALL BE: "HINGED-TYPE" B+DIRECTIONAL DELINEATORS FROM TDOT'S CURRENT QPL. MOUNTED TO ALL GUARDRAIL POSTS EXCEPT END TERMINAL SECTIONS. ATTACHED TO GUARDRAIL POSTS EXCEPT END TERMINAL SECTIONS. USE MANUFACTURER'S RECOMMENDED ADHESIVE
549	99		
PUTNAM CO. PROPOSED GUARDRAIL (RESURFACING)			
QTY (EACH)		ITEM NO. 705-04.20	COMMENTS
WHITE	YELLOW	GUARDRAIL DELINEATION ENHANCEMENT (B+DIR) EACH	DELINEATORS SHALL BE: "HINGED-TYPE" B+DIRECTIONAL DELINEATORS FROM TDOT'S CURRENT QPL. MOUNTED TO ALL GUARDRAIL POSTS EXCEPT END TERMINAL SECTIONS. ATTACHED TO GUARDRAIL POSTS EXCEPT END TERMINAL SECTIONS. USE MANUFACTURER'S RECOMMENDED ADHESIVE
1181	162		

CUMBERLAND CO. BRIDGE DECK RECOMMENDATIONS (RESURFACING)				
BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK RECOMMENDATIONS
18I00400001	0.24R	SR-24	303'	LEAVE AS IS
18I00400002	0.24L	SR-24	307'	LEAVE AS IS
18I00400003	0.950	UNDERPASS DRIPPING SPRINGS RD.	N/A	MAINTAIN MINIMUM VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT

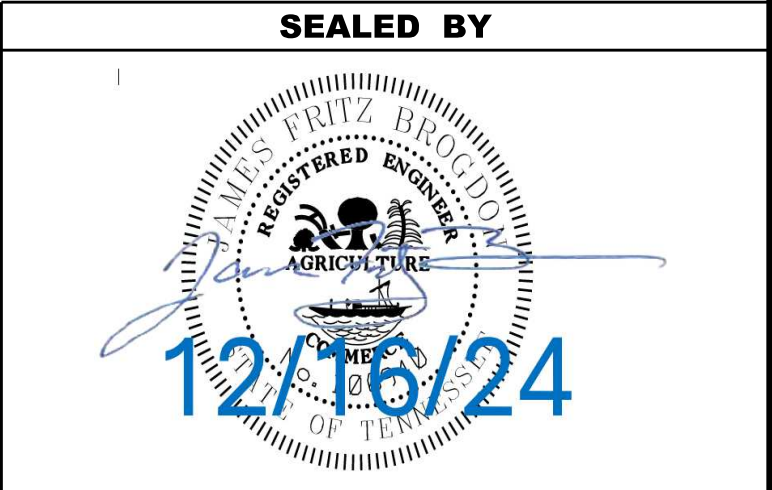
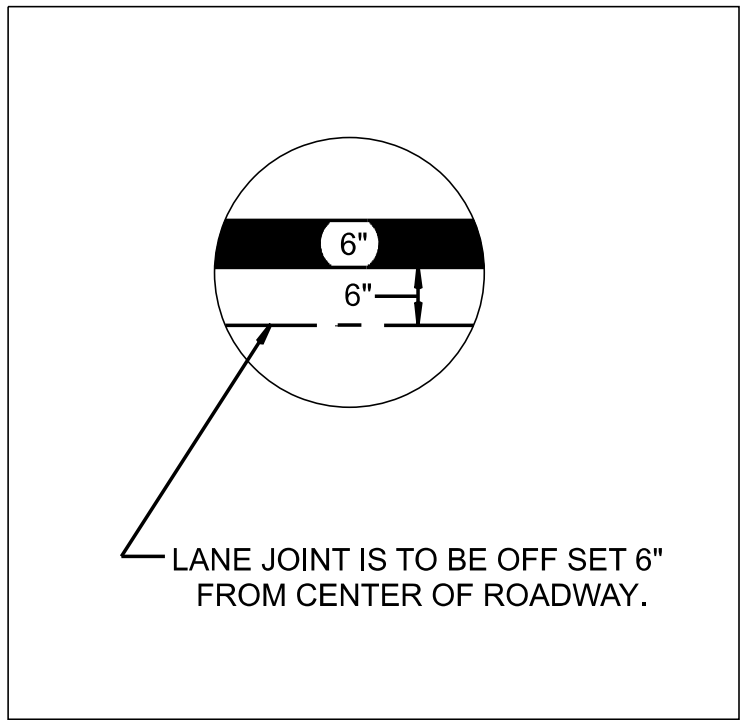
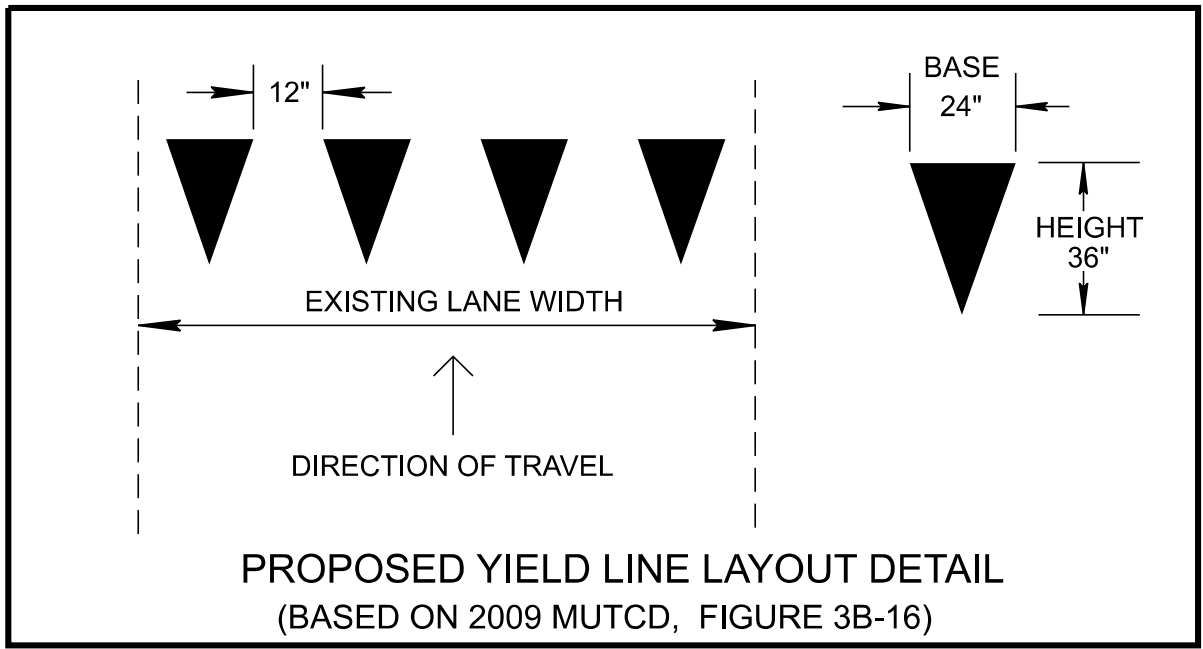
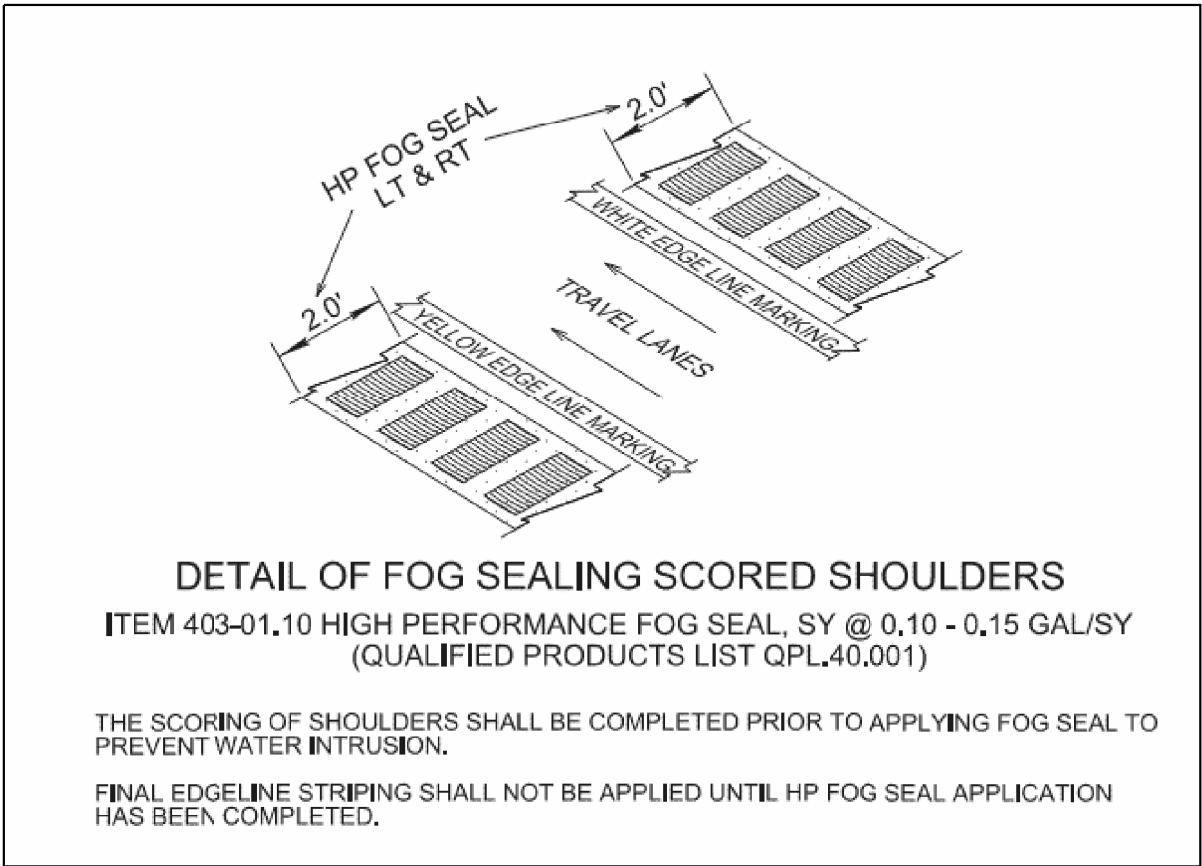
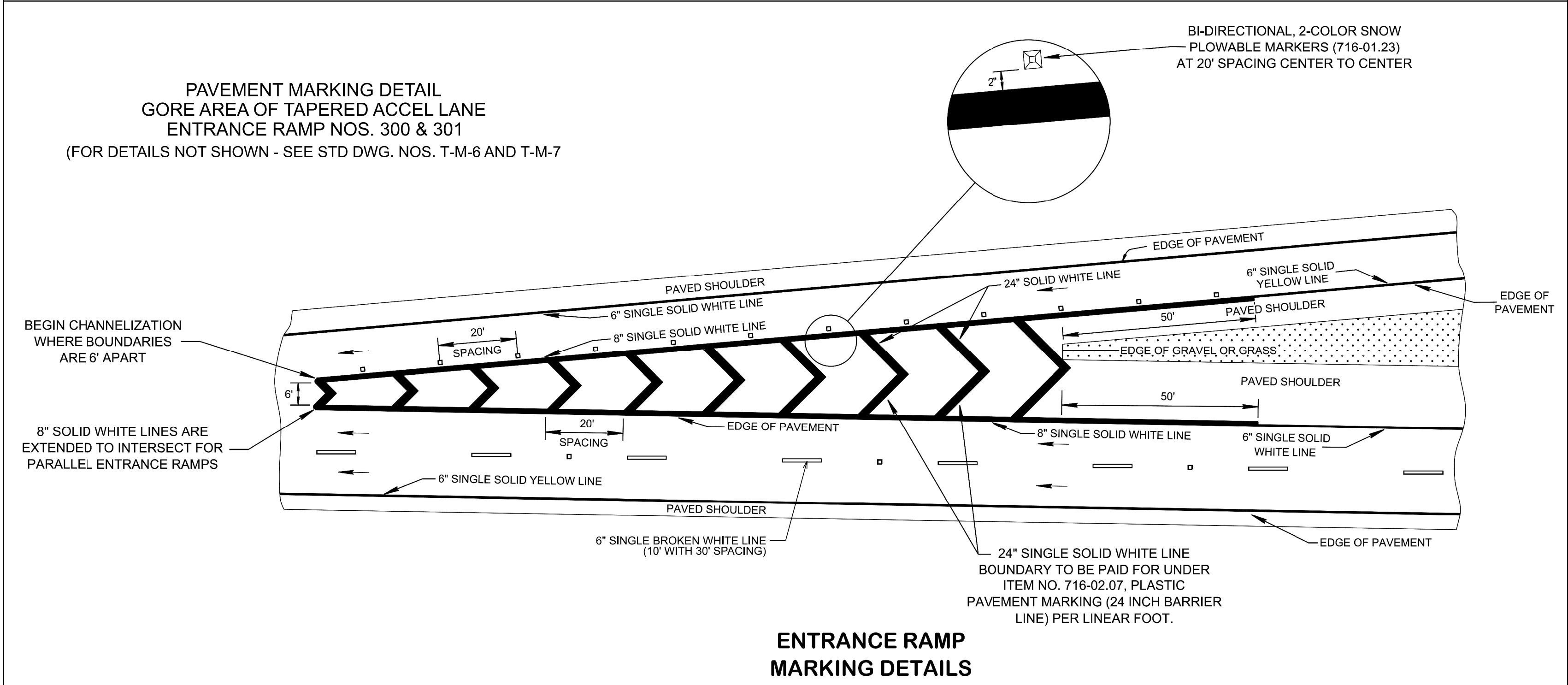
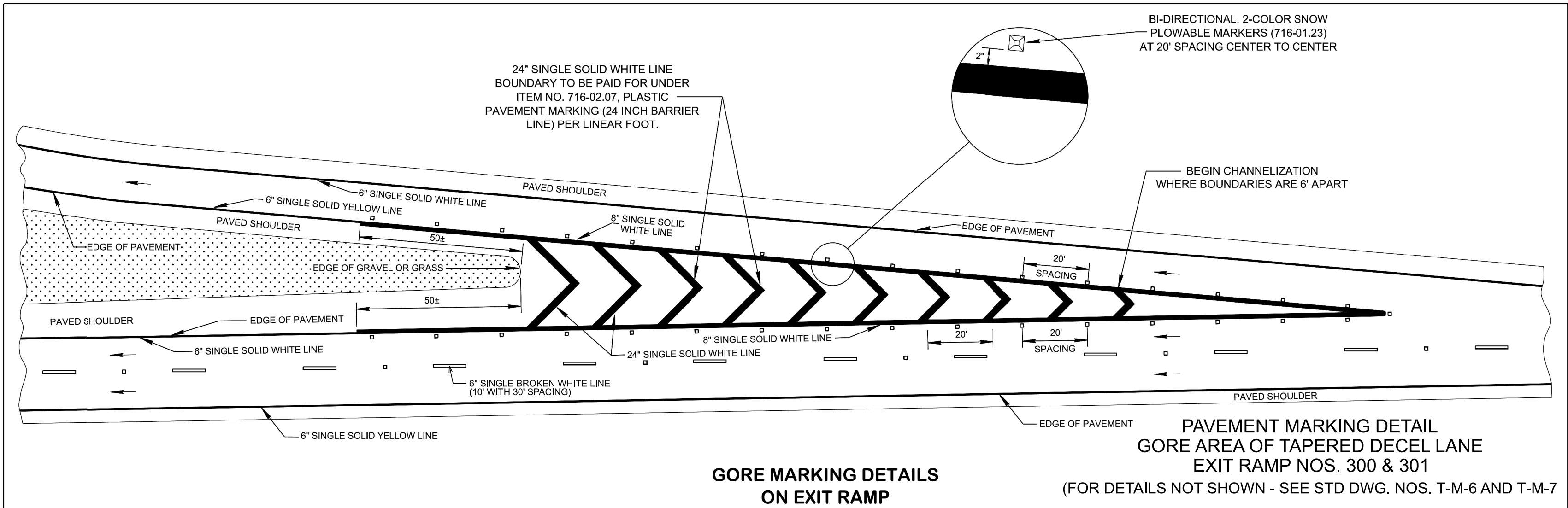
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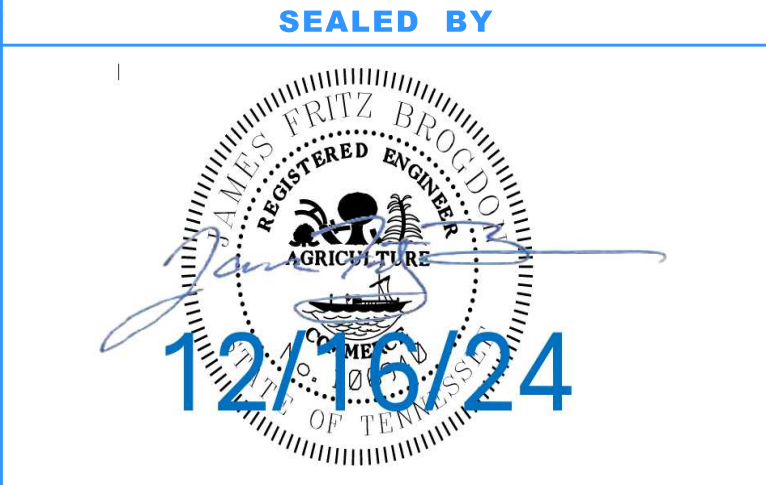
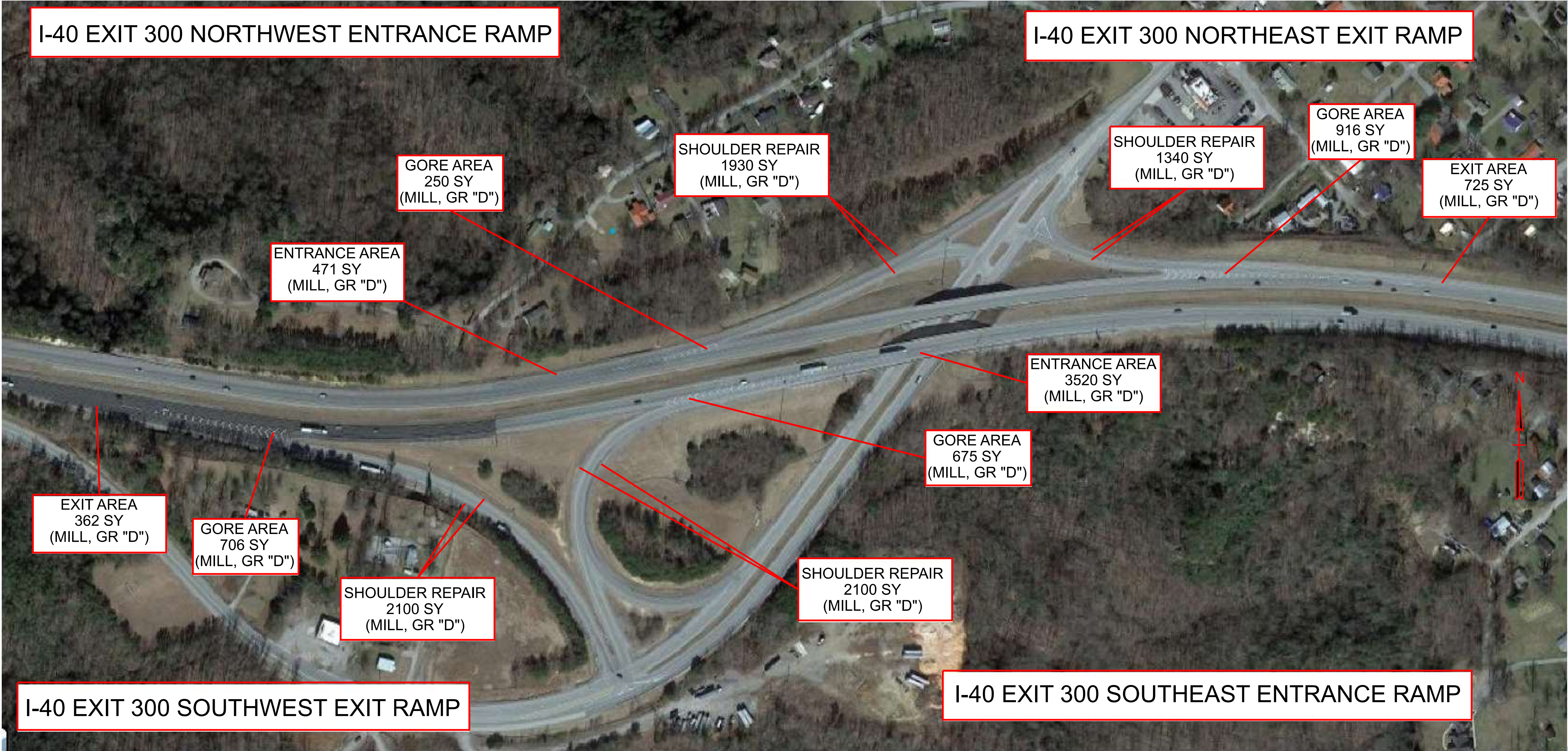
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
QUANTITIES

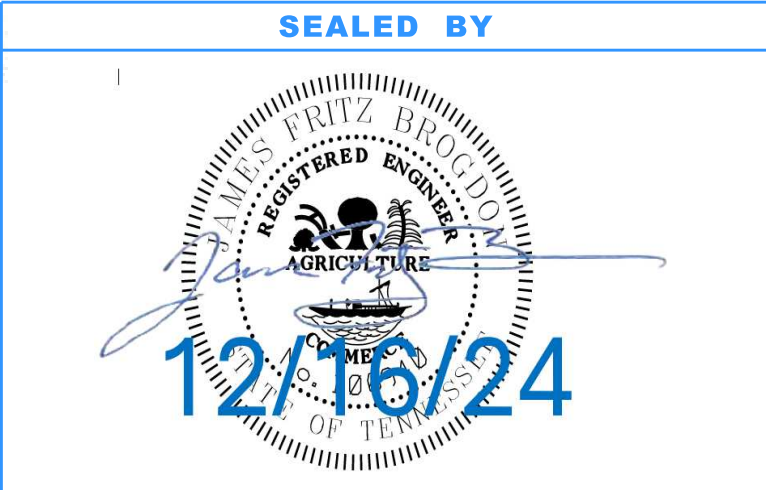
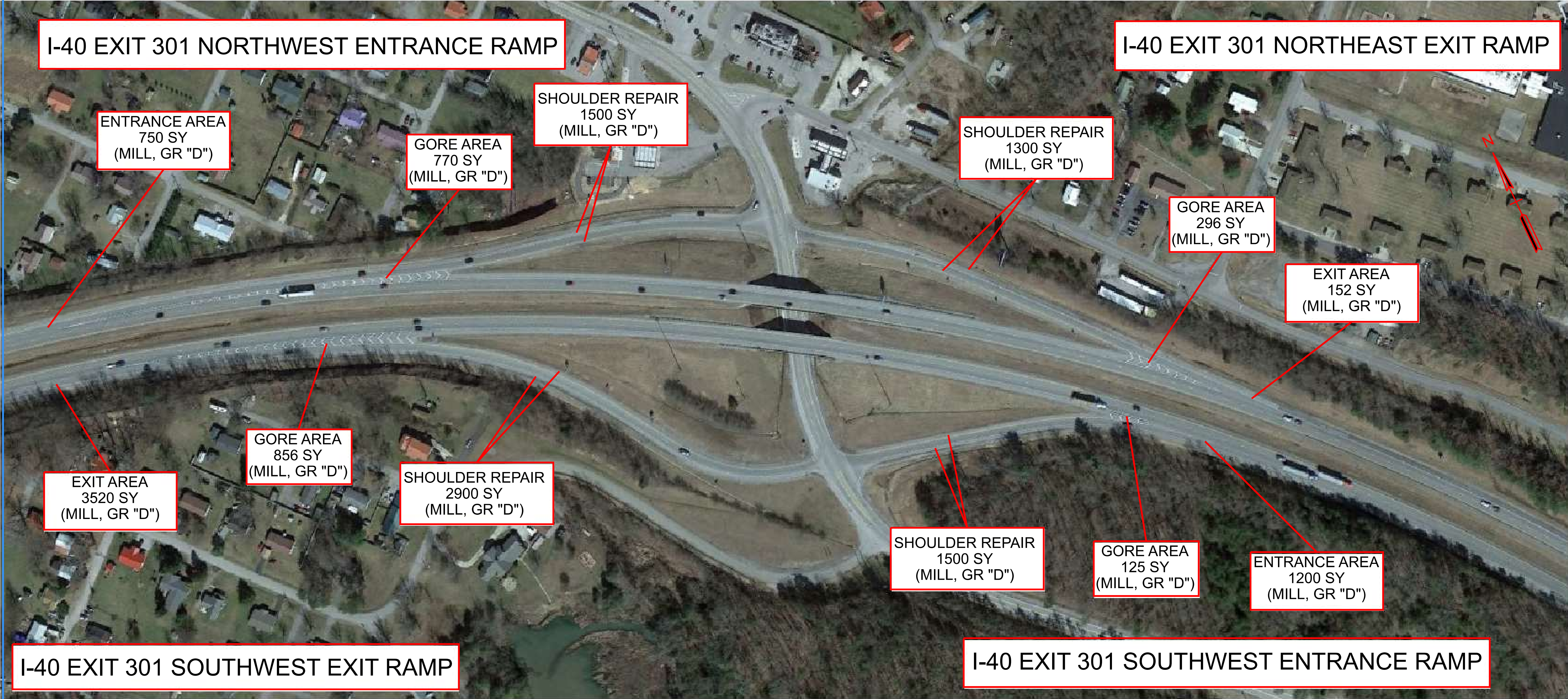
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-40-6(187)	2G
		711040-F8-011	
		711040-M3-011	
		NH-I-40-6(188)	
		181040-F8-009	



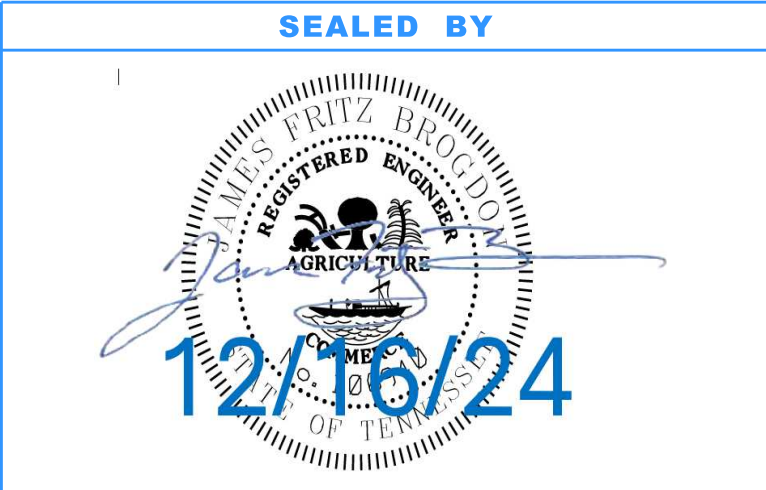
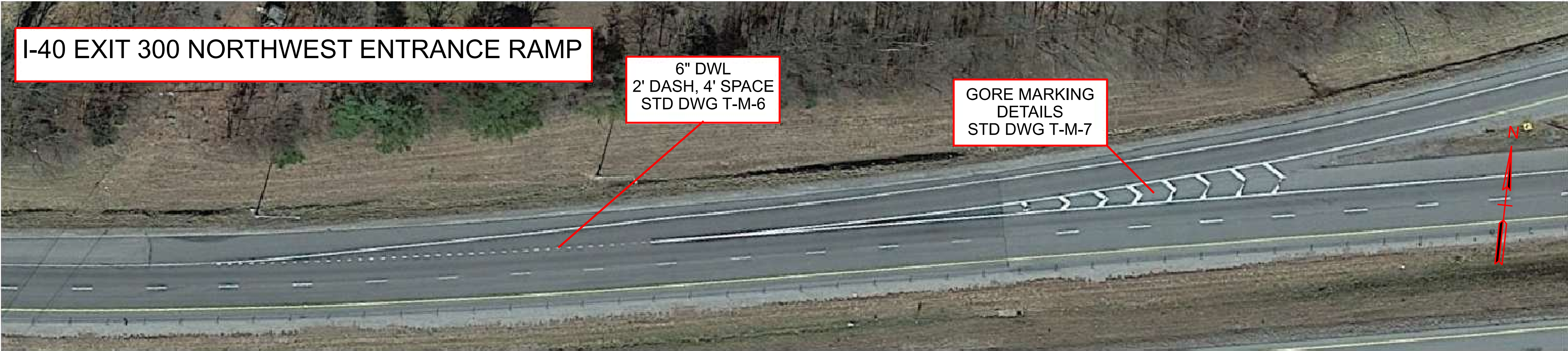
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RESURF.	2025	NH-I-40-6(187)	2G1
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		71I040-M3-011	
		NH-I-40-6(188)	
		18I040-F8-009	



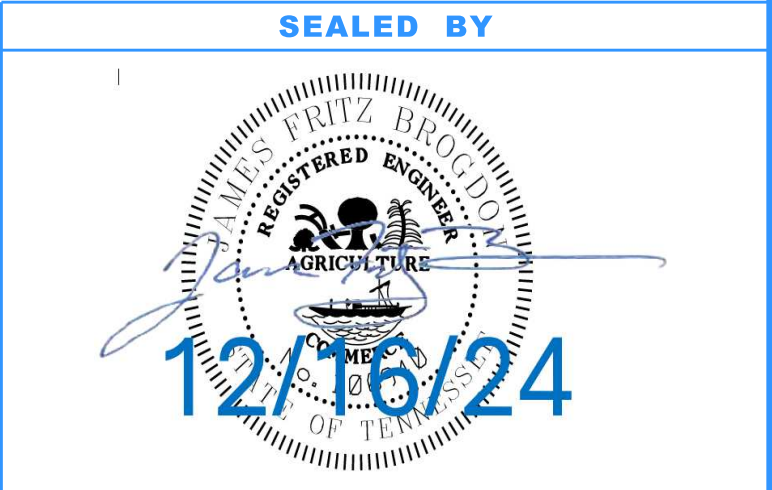
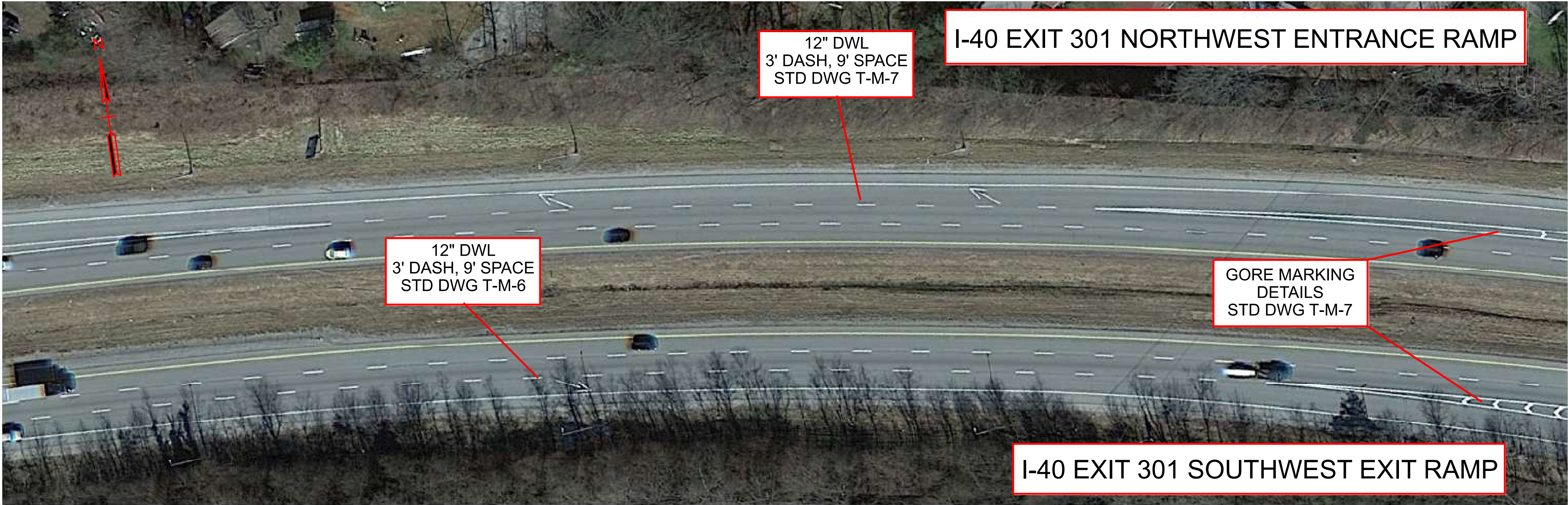
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-40-6(187)	2G2
		71I040-F8-011	
		71I040-M3-011	
		NH-I-40-6(188)	
		18I040-F8-009	



TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-40-6(187)	2G3
		711040-F8-011	
		711040-M3-011	
		NH-I-40-6(188)	
		181040-F8-009	



TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-40-6(187)	2G4
		711040-F8-011	
		711040-M3-011	
		NH-I-40-6(188)	
		181040-F8-009	



UTILITY

- (1)

THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. ABOVE GRADE AND UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM VISIBLE APPURTENANCES AT THE SITE, PUBLIC RECORDS, AND/OR MAPS PREPARED BY OTHERS. THEREFORE, RELIANCE UPON THE TYPE, SIZE, AND LOCATION OF UTILITIES SHOWN SHOULD BE DONE SO WITH THIS CIRCUMSTANCE CONSIDERED. DETAILED VERIFICATION OF EXISTENCE, LOCATION, AND DEPTH SHOULD ALSO BE MADE PRIOR TO ANY DECISION RELATIVE THERETO IS MADE. AVAILABILITY AND COST OF SERVICE SHOULD BE CONFIRMED WITH THE APPROPRIATE UTILITY COMPANY. IN TENNESSEE, IT IS A REQUIREMENT, PER "THE UNDERGROUND UTILITY DAMAGE PREVENTION ACT", THAT ANYONE WHO ENGAGES IN EXCAVATION MUST NOTIFY ALL KNOWN UNDERGROUND UTILITY OWNERS, NO LESS THAN THREE (3) OR NOT MORE THAN TEN (10) WORKING DAYS PRIOR TO THE DATE OF THEIR INTENT TO EXCAVATE AND ALSO TO AVOID ANY POSSIBLE HAZARD OR CONFLICT.
- (2)

UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800- 351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.
- (3)

THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4)

PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5)

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS (PUTNAM)

CATV/FIBER:
CHARTER COMMUNICATIONS
851 SOUTH WILLOW AVENUE, SUITE 206
COOKEVILLE, TN 38501
CONTACT: MORGAN WILCHER
Email: Morgan.Wilcher@charter.com

ELECTRIC:
COOKEVILLE ELECTRIC DEPARTMENT
55 WEST DAVIS ROAD
COOKEVILLE, TN 38506
CONTACT: CARL HANEY
OFFICE PHONE: (931) 520-5400
Email: chaney@cookeville-tn.org

ELECTRIC:
TENNESSEE VALLEY AUTHORITY
1101 MARKET STREET MR-4G
CHATTANOOGA, TN 37402
CONTACT: STEPHEN WILLIAMS
Email: sewilliams@tva.gov

FIBER OPTICS:
TWIN LAKES
PO BOX 67
GAINESBORO, TN 38562
CONTACT: JAMES DOBBS
OFFICE PHONE: (931) 268-2151
Email: jdobbs@twinlakes.net

FIBER OPTICS:
UPPER CUMBERLAND ELECTRIC MEMBERSHIP CORP.
PO BOX 159
CARTHAGE, TN 37030-0159
CONTACT: WAYNE ANDERSON
OFFICE PHONE: (615) 281-8810
Email: wanderson@ucemc.com

GAS:
COOKVILLE GAS DEPARTMENT
16 NORTH OAK AVENUE
COOKEVILLE, TN 38501
CONTACT: AMBER GREENWOOD
OFFICE PHONE: (931) 520-5230
Email: agreenwood@cookeville-tn.org

GAS:
MIDDLE TENNESSEE NATURAL GAS
PO BOX 670
SMITHVILLE, TN 37166
CONTACT: MATT STENNETT
OFFICE PHONE: (931) 754-3515
Email: mstennett@mtng.com

SEWER:
COOKEVILLE WATER QUALITY DEPARTMENT
1860 SOUTH JEFFERSON AVENUE
COOKEVILLE, TN 38506
CONTACT: BARRY TURNER
OFFICE PHONE: (931) 520-5258
Email: bturner@cookeville-tn.org

TELEPHONE:
FRONTIER/CITIZENS COMMUNICATIONS COMPANY
2104 WEST EMORY ROAD
POWELL, TN 37849
CONTACT: JIM HEATHERLY
OFFICE PHONE: (865) 947-8260
Email: james.heatherly@ftr.com

WATER:
COOKEVILLE WATER QUALITY DEPARTMENT
1860 SOUTH JEFFERSON AVENUE
COOKEVILLE, TN 38506
CONTACT: BARRY TURNER
OFFICE PHONE: (931) 520-5258
Email: bturner@cookeville-tn.org

UTILITY OWNERS (CUMBERLAND)

CATV/FIBER:
CHARTER COMMUNICATIONS
851 SOUTH WILLOW AVENUE, SUITE 206
COOKEVILLE, TN 38501
CONTACT: MORGAN WILCHER
Email: Morgan.Wilcher@charter.com

ELECTRIC:
VOLUNTEER ENERGY COOPERATIVE
PO BOX 277
DECATUR, TN 37322
CONTACT: MATTHEW TEAGUE
OFFICE PHONE: (423) 334-7040
Email: mteague@vec.org

FIBER OPTICS:
VOLUNTEER ENERGY COOPERATIVE
PO BOX 277
DECATUR, TN 37322
CONTACT: MATTHEW TEAGUE
OFFICE PHONE: (423) 334-7040
Email: mteague@vec.org

FIBER OPTICS:
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GAINESBORO, TN 38562
CONTACT: JAMES DOBBS
OFFICE PHONE: (931) 268-2151
Email: jdobbs@twinlakes.net

GAS:
MIDDLE TENNESSEE NATURAL GAS
PO BOX 670
SMITHVILLE, TN 37166
CONTACT: MATT STENNETT
OFFICE PHONE: (931) 754-3515
Email: mstennett@mtng.com

TELEPHONE:
BELLSOUTH DBA AT&T
300 EAST MARTIN LUTHER KING BOULEVARD, 5TH FLOOR
CHATTANOOGA, TN 37403
CONTACT: JOE PERREL
OFFICE PHONE: (423) 266-1566
Email: jp1389@att.com

TELEPHONE:
BEN LOMAND CONNECT
PO BOX 670
MCMINNVILLE, TN 37111
CONTACT: RICHARD BOYD
OFFICE PHONE: (931) 668-6692
Email: richardboyd@benlomand.org

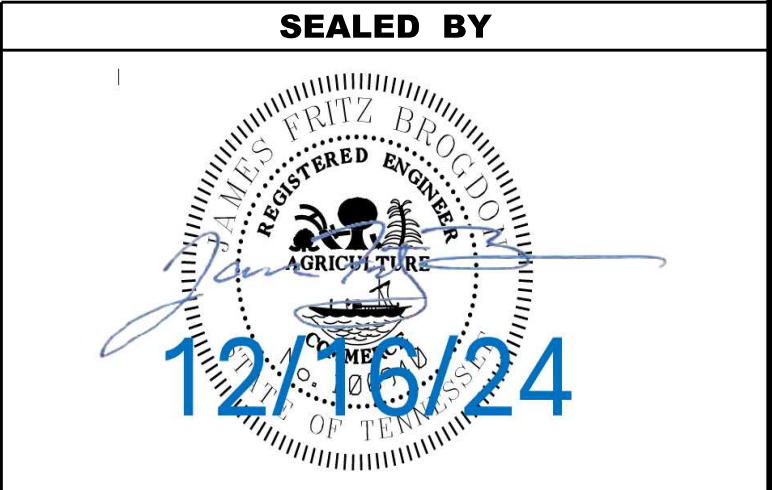
TELEPHONE:
FRONTIER/CITIZENS COMMUNICATIONS COMPANY
250 SOUTH FRANKLIN AVENUE
COOKEVILLE, TN 38501
CONTACT: STEVE WORLEY
OFFICE PHONE: (931) 646-4534
Email: steve.worley@ftr.com

TELEPHONE:
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PO BOX 67
GAINESBORO, TN 38562
CONTACT: JAMES DOBBS
OFFICE PHONE: (931) 268-2151
Email: jdobbs@twinlakes.net

WATER:
CRAB ORCHARD UTILITY DISTRICT
2089 EAST 1ST STREET
CROSSVILLE, TN 38555-4659
CONTACT: EVERETT BOLIN
OFFICE PHONE: (931) 484-6987
Email: ebolin.coud@frontiernet.net

WATER:
CITY OF CROSSVILLE
392 NORTH MAIN STREET
CROSSVILLE, TN 38555-4232
CONTACT: TIM BEGLEY
OFFICE PHONE: (931) 456-5680
Email: tim.begley@crossvilletn.gov

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-L40-6(187)	3
		71I040-F8-011	
		71I040-M3-011	
		NH-L40-6(188)	
		18I040-F8-009	



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

UTILITY NOTES
AND
UTILITY OWNERS

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:
- a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
- a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

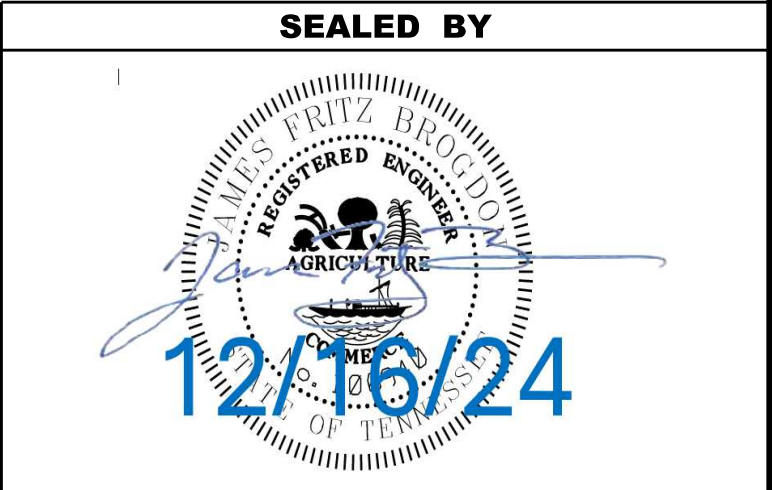
(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a. PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-L-40-6(187)	T1
		71I040-F8-011	
		71I040-M3-011	
		NH-L-40-6(188)	
		18I040-F8-009	



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-40-6(187)	T2
		711040-F8-011	
		711040-M3-011	
		NH-I-40-6(188)	
		181040-F8-009	

711040-F8-001
711040-M3-001
BEGIN PROJECT NO. NH-I-40-6(187) RESURFACE & SAFETY
L.M. 33.32

DIGITAL SPEED LIMIT ASSEMBLY
APPROX. LOCATION LM 33.52 (I-40 EB)
(AFTER EXIT 300 SE ENTRANCE RAMP)

DIGITAL SPEED LIMIT ASSEMBLY
APPROX. LOCATION LM 32.32 (I-40 EB)
(1 MILE PRIOR TO BEGIN PROJECT)

DIGITAL SPEED LIMIT ASSEMBLY
APPROX. LOCATION LM 34.45 (I-40 EB)
(AFTER EXIT 301 SE ENTRANCE RAMP)

DIGITAL SPEED LIMIT ASSEMBLY
APPROX. LOCATION LM 36.45 (I-40 EB)
(AT APPROX. 2 MILE SPACING)

711040-F8-001
711040-M3-001
END PROJECT NO. NH-I-40-6(187) RESURFACE & SAFETY
L.M. 37.09

181040-F8-009
BEGIN PROJECT NO. NH-I-40-6(188) RESURFACE & SAFETY
L.M. 0.00

DIGITAL SPEED LIMIT ASSEMBLY
APPROX. LOCATION LM 1.36 (I-40 EB)
(AT APPROX. 2 MILE SPACING)

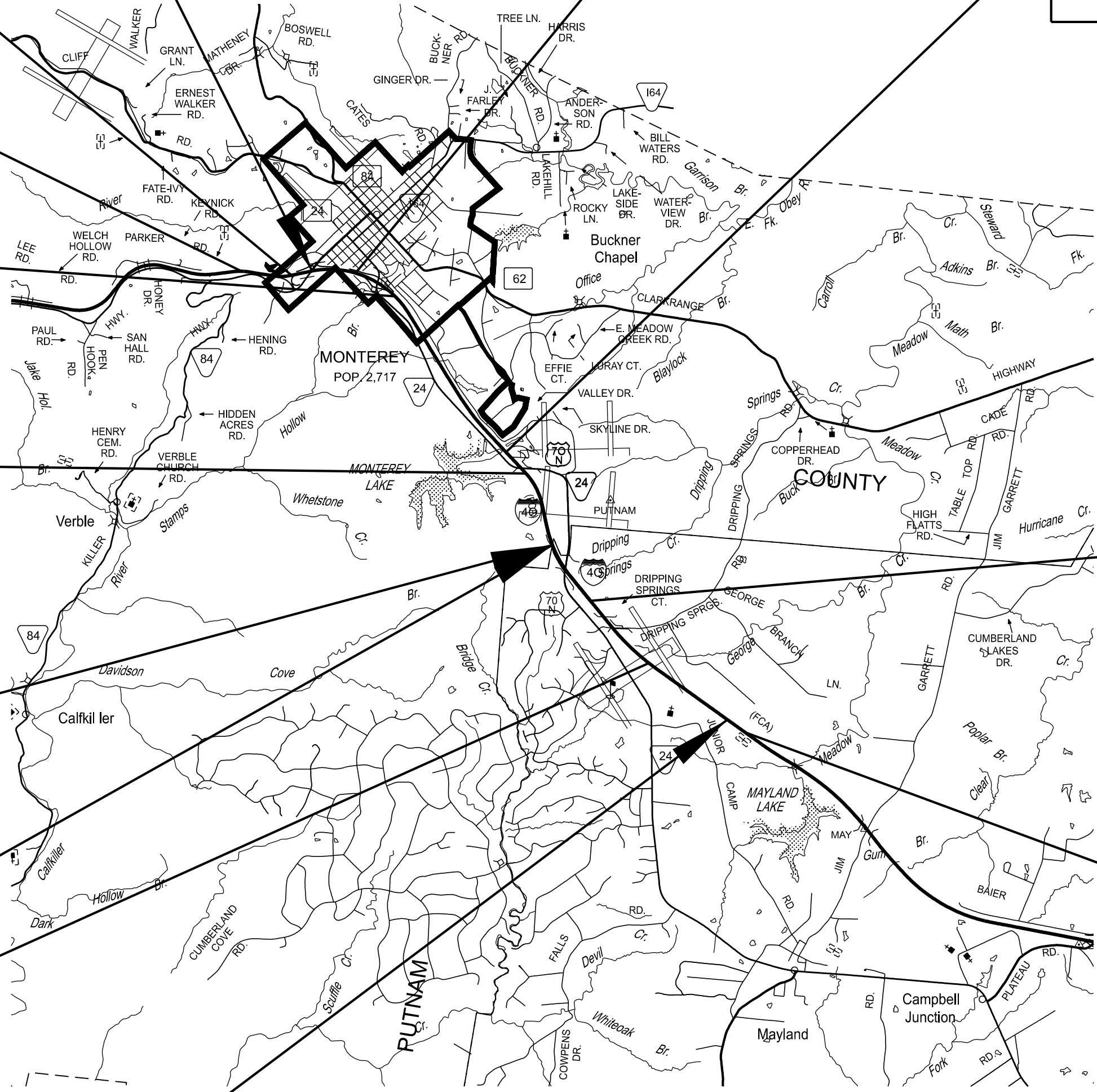
181040-F8-009
END PROJECT NO. NH-I-40-6(188) RESURFACE & SAFETY
L.M. 2.00

DIGITAL SPEED LIMIT ASSEMBLY
APPROX. LOCATION LM 33.71 (I-40 WB)
(AFTER EXIT 301 NW ENTRANCE RAMP)

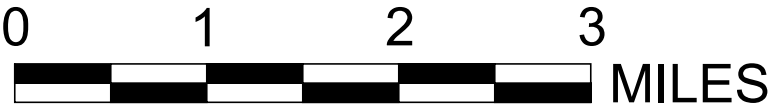
DIGITAL SPEED LIMIT ASSEMBLY
APPROX. LOCATION LM 36.09 (I-40 WB)
(AT APPROX. 2 MILE SPACING)

DIGITAL SPEED LIMIT ASSEMBLY
APPROX. LOCATION LM 1.00 (I-40 WB)
(AT APPROX. 2 MILE SPACING)

DIGITAL SPEED LIMIT ASSEMBLY
APPROX. LOCATION LM 3.00 (I-40 WB)
(1 MILE PRIOR TO BEGIN PROJECT)



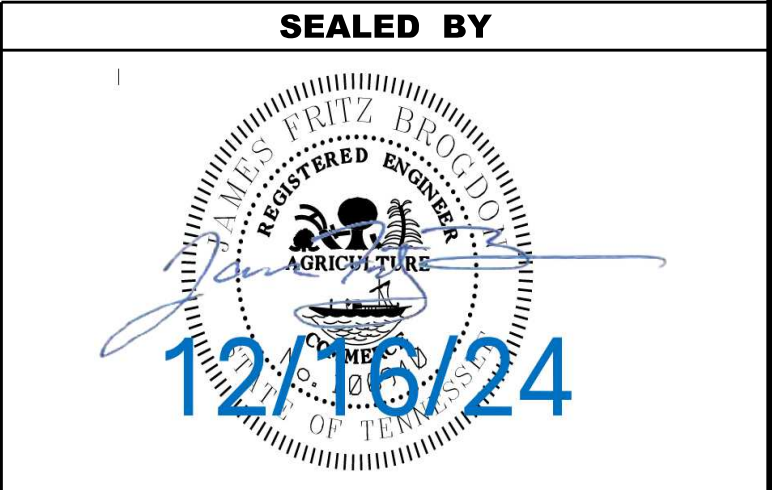
SCALE: 1"= 5280'



DETAIL OF SUGGESTED LOCATIONS OF DIGITAL SPEED LIMIT ASSEMBLY

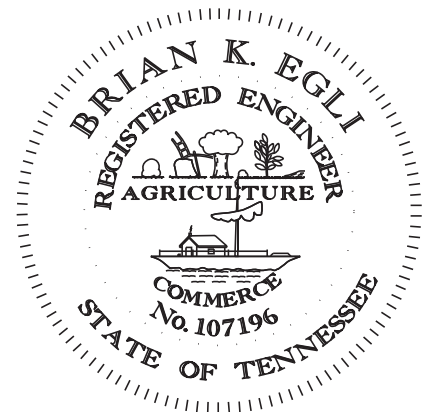
- (A) THE CONTRACTOR SHALL MONITOR ALL SPEED LIMIT ASSEMBLIES TO PREVENT COMMUNICATING CONFLICTING MESSAGES CONCERNING THE SPEED LIMIT.
- (B) EXISTING SPEED LIMIT SIGNS SHALL BE SUFFICIENTLY COVERED THROUGH THE ENTIRE WORK ZONE. IF THE COVERING BEGINS TO REVEAL THE EXISTING SPEED LIMIT SIGN, THE COVERING SHALL BE REPAIRED OR REPLACED AS DIRECTED BY THE TDOT PROJECT ENGINEER. SEE SPECIAL PROVISION 712DSL.
- (C) TAPE SHALL NOT BE AFFIXED TO THE REFLECTIVE SHEETING.

THE ESTIMATED TIME REQUIRED FOR THE QUEUE TRUCK PROTECTION IS 500 HOURS.
TO BE PAID FOR UNDER ITEM 712-08.12 QUEUE PROTECTION TRUCK, 42 DAYS TOTAL.
SEE SPECIAL PROVISION 712PTQ FOR INFORMATION.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC
CONTROL
PLANS



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Brian Egli

2025.01.07 13:24:13 -06'00'

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

TENNESSEE DEPARTMENT OF TRANSPORTATION
505 DEADERICK STREET, SUITE 1200
NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME

SHEET NO.

SIGNATURE SHEET _____ STRUCTURE-SIGN 1
BRIDGE PLANS _____ B1 THRU B10

YEAR	PROJECT NO.	SHEET NO.			
2025	711040-M3-011	STRUCTURE-SIGN 1			
STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION					
SIGNATURE SHEET					

<u>INDEX OF DRAWINGS</u>	<u>DWG. NO.</u>	<u>LAST REV. DATE</u>
SIGNATURE SHEET	STRUCTURE-SIGN 1	
INDEX OF DRAWINGS	B1	
BRIDGE TABULATION AND ESTIMATED QUANTITIES	B2	
BRIDGE DECK REPAIR NOTES	B3	
CONCRETE OVERLAYED BRIDGE DECK REPAIR NOTES	B4	
APPROACH SLAB REPAIR DETAIL NOTES	B5	
BRIDGE DECK SEALANT DETAIL NOTES	B6	
PLAN VIEW (71100400053)	B7	
PHASE CONSTRUCTION	B8	
PLAN VIEW (71100400056)	B9	
PHASE CONSTRUCTION	B10	

<u>INDEX OF REFERENCE DRAWINGS</u>	<u>DWG. NO.</u>
LAYOUT OF BRIDGE TO BE REPAIRED	BR-124-17
SUPERSTRUCTURE DETAILS	BR-124-19
LAYOUT OF BRIDGE TO BE REPAIRED	BR-124-25
SUPERSTRUCTURE DETAILS	BR-124-26

01/06/25

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION
INDEX OF DRAWINGS
71-140-33.46R
71-140-34.12L
OVER
SR-24
BR. NOS. 71I00400053
71I00400056
PUTNAM COUNTY
2025

B1

PIN 132593.00

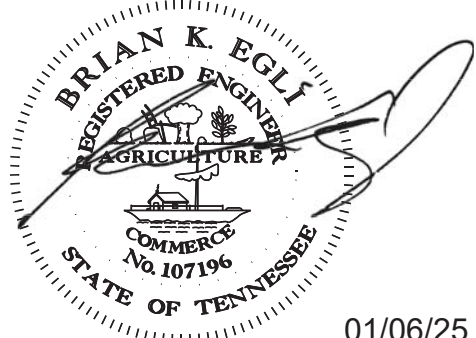
PROJECT NO.		YEAR	SHEET NO.
711040-M3-011		2025	B2
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

NOTE: PARTIAL DEPTH DECK & APPROACH REPAIR AREA WILL BE LOCATED AFTER THE EXISTING ASPHALT IS REMOVED FOR 71-I40-33.46R & 71-I40-34.12L

TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES						
LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF THE SLAB) S.Y.	604-10.53 CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT) S.Y.	604-10.56 CONCRETE OVERLAYED BRIDGE REPAIR (PARTIAL DEPTH) S.Y.	617-01 BRIDGE DECK SEALANT S.Y.
71-I40-33.46R OVER SR-24 (71I00400053)	BR-124-17 BR-124-19	CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PVMT.) BRIDGE DECK REPAIR (PARTIAL DEPTH OF THE SLAB) CONCRETE OVERLAYED BRIDGE REPAIR (PARTIAL DEPTH) BRIDGE DECK SEALANT	10	10	10	1672
71-I40-34.12L OVER SR-24 (71I00400056)	BR-124-25 BR-124-26	CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PVMT.) CONCRETE OVERLAYED BRIDGE REPAIR (PARTIAL DEPTH) BRIDGE DECK SEALANT		10	10	664
TOTAL			10	20	20	2336

NOTE: THE EXISTING MEMBRANE SEAL AND ASPHALT SHALL BE REMOVED FROM THE BRIDGE DECK BEFORE MARKING AREAS FOR REPAIR AND NEW MEMBRANE SEAL IS APPLIED.

THE COST IS INCIDENTAL TO OTHER ITEMS BID ON.

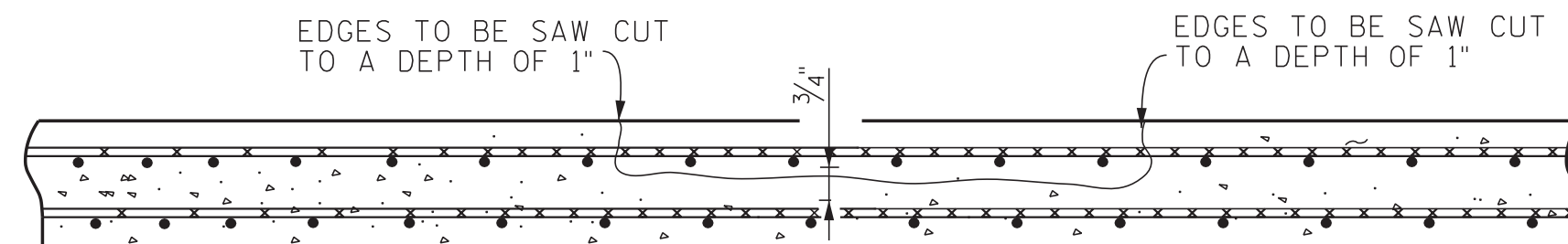


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE TABULATION AND
ESTIMATED QUANTITIES
71-I40-33.46R
71-I40-34.12L
OVER
SR-24
BR. NOS. 71I00400053
71I00400056
PUTNAM COUNTY
2025

DESIGNED BY _____ DATE _____
DRAWN BY Z.HAYNES DATE 9/24
SUPERVISED BY K. MARTINKO DATE 9/24
CHECKED BY _____ DATE _____

USE AN EXTENDED RAPID SET CEMENTITIOUS
PATCHING MATERIALS (FOR PATCHING MATERIAL
REFER TDOT QUALIFIED PRODUCT LIST NO.13.004)



NOTE: PARTIAL DEPTH DECK REPAIR ONLY REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF $\frac{3}{4}$ " BELOW THE BOTTOM BAR OF THE TOP MAT OF REINFORCING STEEL.

NOTE: PARTIAL DEPTH DECK REPAIRS:
ALL REINFORCING STEEL IN THE AREAS OF THE DECK REPAIRS SHALL BE COMPLETELY CLEANED.
CLEANING SHALL BE DONE PRIOR TO PLACING NEW CONCRETE. AREAS OF CONCRETE REMOVAL
SHALL BE DESIGNATED BY PERSONNEL FROM THE HEADQUARTERS, BRIDGE INSPECTION AND REPAIR
OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE
BRIDGE REPAIR OFFICE AT LEAST (3) DAYS IN ADVANCE.

THE COST OF CONCRETE, CONCRETE REMOVAL, LABOR, CLEANING REBARS AND ANY MISCELLANEOUS MATERIAL REQUIRED SHALL BE PAID FOR UNDER ITEM NO. 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB), S.Y.

NOTE: ITEM NO. 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB)
MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

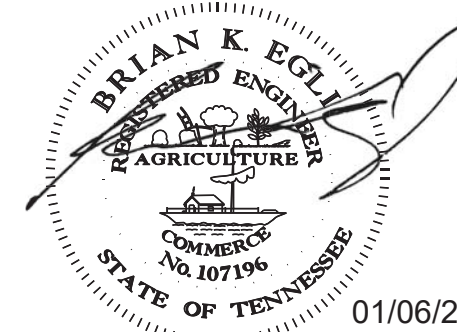
POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE
IN MAKING PARTIAL DEPTH REPAIRS ARE SUBJECT TO THE
FOLLOWING RESTRICTIONS:

(1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS
HEAVIER THAN NOMINAL 60 lb. CLASS SHALL NOT BE USED. TRAFFIC
CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC.

(2) CHIPPING HAMMERS OF THE 15 lb. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

REPAIRS SHALL USE AN EXTENDED NON-MAGNESIUM PHOSPHATE PRODUCT FROM THE TDO QUALIFIED PRODUCTS LIST:
13.004-RAPID SET CEMENTITIOUS PATCHING MATERIALS.
MATERIAL SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS.
MIX MUST MEET 3000 PSI BEFORE OPENING TO TRAFFIC.

DESIGNED BY _____ DATE _____
 DRAWN BY _____ Z.HAYNES DATE 9/24
 SUPERVISED BY _____ K. MARTINKO DATE 9/24
 CHECKED BY _____ DATE _____

[illegible]

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE DECK
REPAIR NOTES
71-I40-33.46R
71-I40-34.12L
OVER
SR-24
BR. NOS. 71I00400053
71I00400056
PUTNAM COUNTY
2025

PROJECT NO.	YEAR	SHEET NO.
711040-M3-011	2025	B4

[illegible]

DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 9/24
 SUPERVISED BY K. MARTINKO DATE 9/24
 CHECKED BY _____ DATE _____

CONCRETE OVERLAYED
BRIDGE DECK
REPAIR NOTES
71-I40-33.46R
71-I40-34.12L
OVER
SR-24
BR. NOS. 71I00400053
71I00400056
PUTNAM COUNTY
2025

[illegible]

B5

[illegible]

[illegible]

SPECIAL NOTE: THE CONTRACTOR SHOULD TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK, SO AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.

PLAN VIEW
71-140-33.46R
OVER
SR-24
BR. NO. 71I00400053
PUTNAM COUNTY
2025

B7

PROJECT NO.	YEAR	SHEET NO.
71I040-M3-011	2025	B8

[illegible]

(71-140-33.46R)

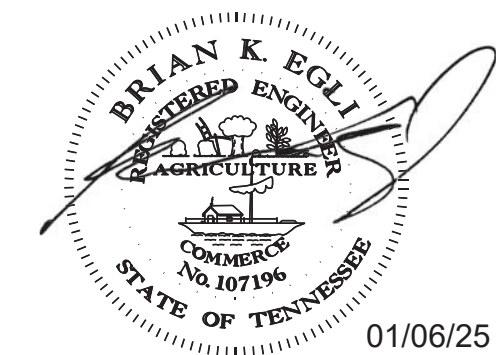
(LOOKING AHEAD ON SURVEY)



(71-140-33.46R)

(LOOKING AHEAD ON SURVEY)

DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 9/24
 SUPERVISED BY K. MARTINKO DATE 9/24
 CHECKED BY _____ DATE _____



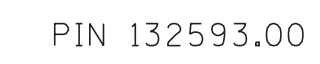
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
71-140-33.46R
OVER
SR-24
BR. NO. 71I00400053
PUTNAM COUNTY
2025

[illegible]

SPECIAL NOTE: THE CONTRACTOR SHOULD TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK, SO AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.

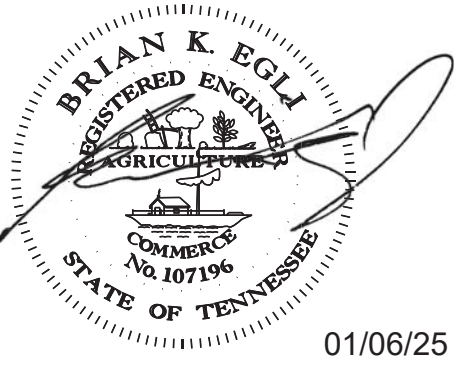
PLAN VIEW
71-140-34.12L
OVER
SR-24
BR. NO. 71I00400056
PUTNAM COUNTY
2025

B9

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(71-I40-34.12L)

(LOOKING AHEAD ON SURVEY)

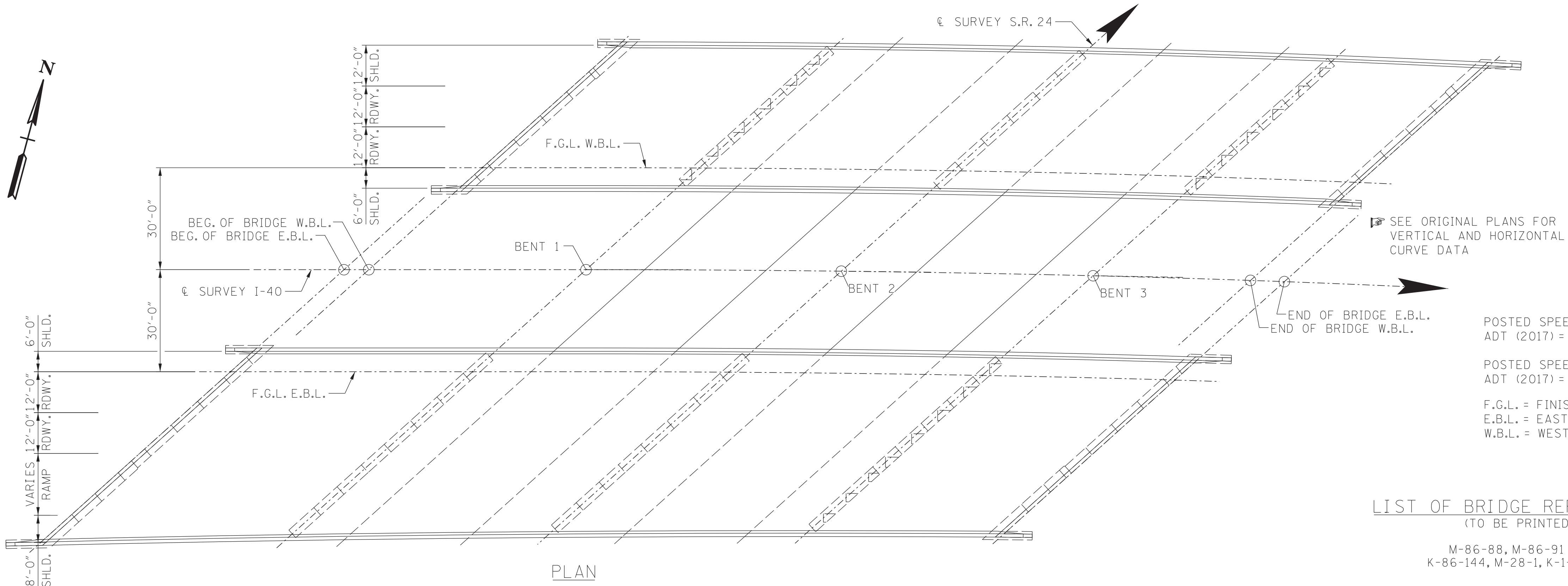
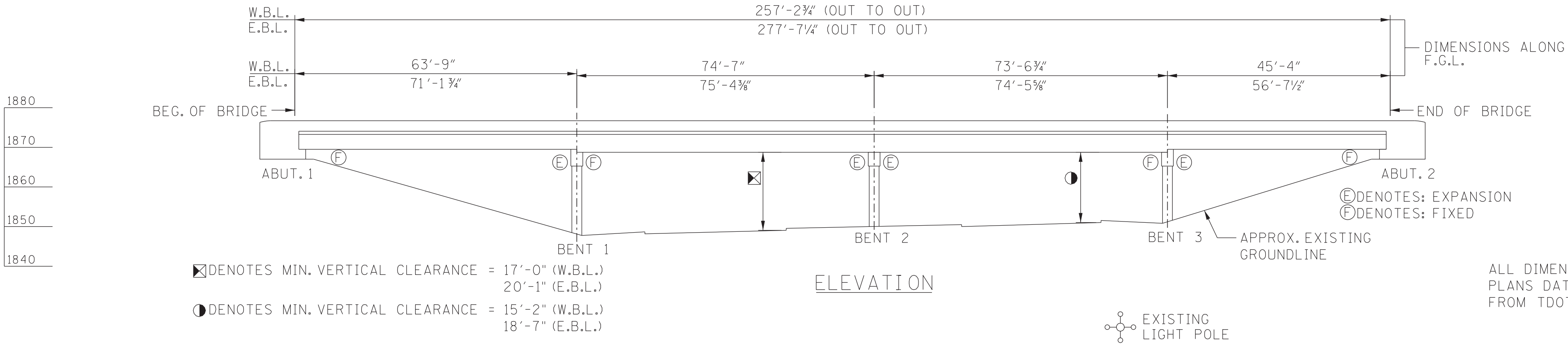


(71-I40-34.12L)

(LOOKING AHEAD ON SURVEY)

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
71-140-34.12L
OVER
SR-24
BR. NO. 71I00400056
PUTNAM COUNTY
2025

PROJECT NO.		YEAR	SHEET NO.
71100-4123-04		2017	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	01-18-17	MDS	UPDATED LAST REV. DATE



LIST OF BRIDGE REFERENCE DRAWINGS
(TO BE PRINTED WITH PLANS)

M-86-88, M-86-91 THRU M-86-102,
K-86-144, M-28-1, K-1-755 THRU K-1-768

SCOPE OF WORK

1. PROVIDE REQUIRED TRAFFIC CONTROL AND PHASED CONSTRUCTION MAINTAINING A MINIMUM OF ONE LANE OF TRAFFIC ON EACH BRIDGE.
2. PROVIDE TRAFFIC CONTROL FOR SR 24 (BELOW).
3. REMOVE EXISTING ASPHALT OVERLAY ON BRIDGES.
4. RECONSTRUCT THE CONCRETE BRIDGE DECKS IN AREAS OF FULL AND PARTIAL DEPTH DECK REPAIR (UTILIZE 18 HOUR CONCRETE).
5. ADD REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS (PABE'S) TO EACH END OF EACH BRIDGE (UTILIZE HIGH EARLY STRENGTH, 10 DAY CONCRETE).
6. PLACE NEW 3 1/4" ASPHALT SEAL WITH MEMBRANE APPLIED DIRECTLY ON BRIDGE DECK AND CONCRETE PABE'S. ASPHALT SHALL CONSIST OF B-MODIFIED AND GRADING "D" MIXES. UTILIZE SNOWFLOWABLE MARKERS AS REQUIRED.
7. REPAIR DAMAGED/DETERIORATED AREAS OF CONCRETE BEAMS.
8. REPAIR DETERIORATED AREAS OF CONCRETE ON DIAPHRAGMS AND SUBSTRUCTURE UNITS.
9. EPOXY-INJECT CRACKS IN BEAMS, DIAPHRAGMS, AND SUBSTRUCTURE UNITS.
10. CONSTRUCT ASPHALT PAVEMENT TRANSITIONS AT ENDS OF PABE.
11. HIGH PRESSURE WATER WASH AND TEXTURE COAT CONCRETE SURFACES OF THE BRIDGE RAILING, DECK OVERHANGS, BOTTOM AND OUTSIDE FACE OF FASCIA BEAMS, BOTTOM OF INTERIOR BEAMS, ALL EXPOSED SURFACES OF THE SUBSTRUCTURES, AND TRAFFIC AND TOP FACES OF PIER PROTECTION ALONG S.R. 24.
12. ARMOR ERODED AREA NEAR ABUTMENT 1 ON THE RIGHT SIDE OF THE RIGHT BRIDGE.
13. ALL DIRT/DEBRIS/TRASH SHALL BE REMOVED FROM SUBSTRUCTURES AND BELOW BRIDGE (COST TO BE INCLUDED IN ITEMS BID ON).
14. REMOVE VEGETATION FOR A DISTANCE OF TEN FEET ALONG EACH SIDE OF THE BRIDGES AS DIRECTED BY THE ENGINEER.
15. THE CONTRACTOR IS TO MAINTAIN ALL MEMORIAL SIGNAGE.

LIST OF BRIDGE DRAWINGS

DRAWING	DRAWING NO.	LAST REV. DATE
LAYOUT OF BRIDGE TO BE REPAIRED (BRIDGE NO. 71-10040-33.46)	BR-124-17	01-18-2017
ESTIMATED BRIDGE QUANTITIES AND NOTES	BR-124-18	
SUPERSTRUCTURE DETAILS (BRIDGE NO. 71-10040-33.46)	BR-124-19	
SUPERSTRUCTURE REPAIR DETAILS (BRIDGE NO. 71-10040-33.46)	BR-124-20	
PAVEMENT AT BRIDGE ENDS (BRIDGE NO. 71-10040-33.46)	BR-124-21	
SUBSTRUCTURE REPAIRS (BRIDGE NO. 71-10040-33.46)	BR-124-22	
SUBSTRUCTURE REPAIRS (BRIDGE NO. 71-10040-33.46)	BR-124-23	
CONCRETE REPAIR DETAILS	BR-124-24	
LAYOUT OF BRIDGE TO BE REPAIRED (BRIDGE NO. 71-10040-34.12)	BR-124-25	01-18-2017
SUPERSTRUCTURE DETAILS (BRIDGE NO. 71-10040-34.12)	BR-124-26	
SUPERSTRUCTURE REPAIR DETAILS (BRIDGE NO. 71-10040-34.12)	BR-124-27	
PAVEMENT AT BRIDGE ENDS (BRIDGE NO. 71-10040-34.12)	BR-124-28	
SUBSTRUCTURE REPAIRS (BRIDGE NO. 71-10040-34.12)	BR-124-29	



DESIGNED BY H.H. NEAL DATE 11/2016
DRAWN BY M.D. SIMPSON DATE 11/2016
SUPERVISED BY G.S. WILSON DATE 11/2016
CHECKED BY G.S. WILSON DATE 11/2016

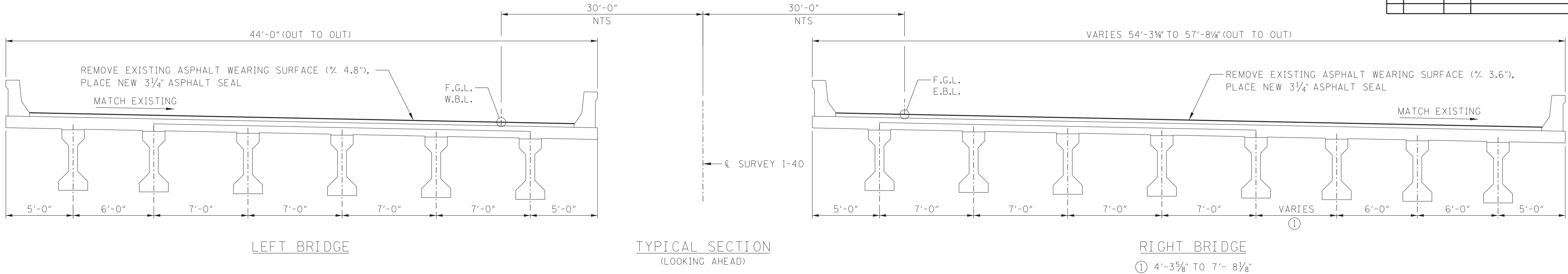


STATE OF TENNESSEE
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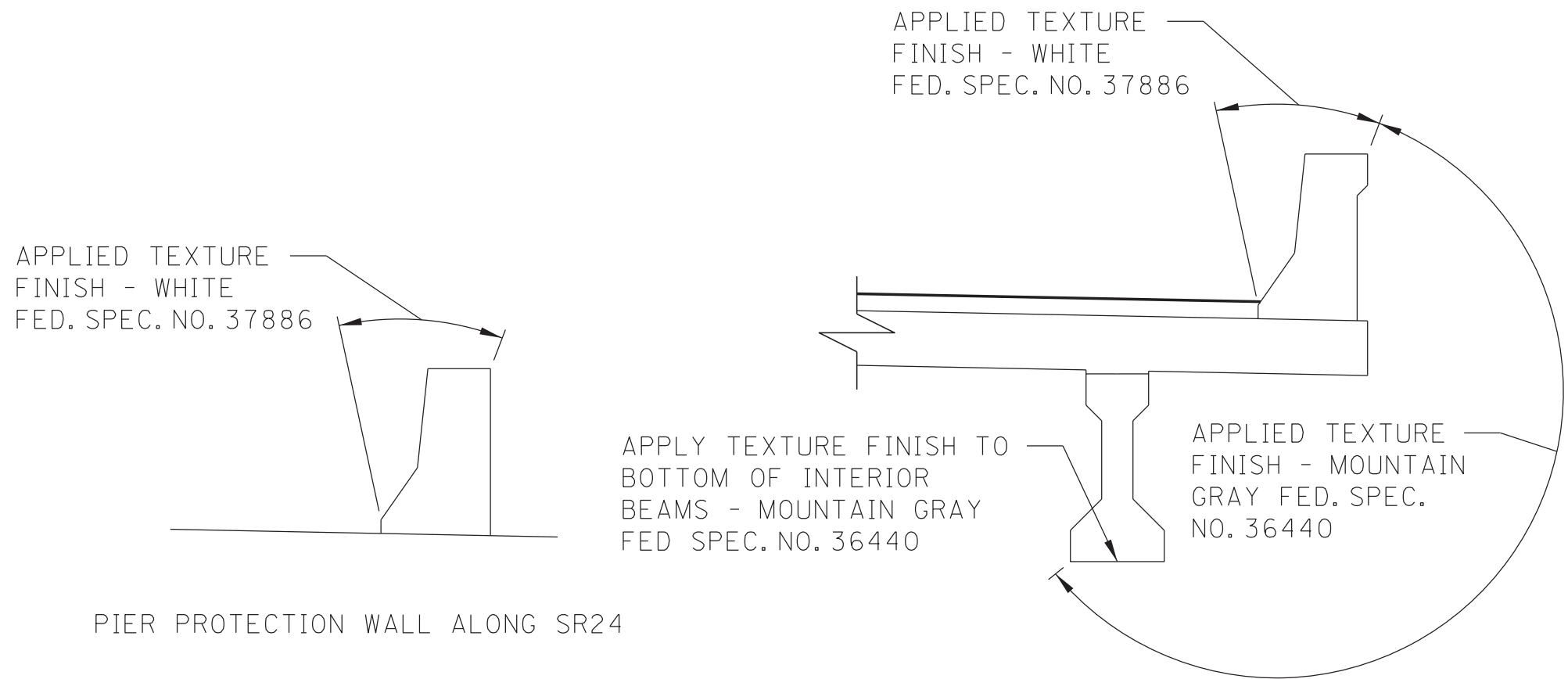
LAYOUT OF
BRIDGE TO BE REPAIRED
BRIDGE NO. 71-I0040-33.46
INTERSTATE 40 (R&L) OVER S.R. 24
PUTNUM COUNTY
2017

BR-124-17

PROJECT NO.	YEAR	SHEET NO.	
71100-4123-04	2017		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



COST OF ASPHALT WEARING SURFACE REMOVAL TO BE INCLUDED IN ITEM NO. 604-10.14, REMOVE EXISTING WEARING SURFACE, L.S.



APPLIED TEXTURE FINISH DETAILS
(LEFT AND RIGHT BRIDGES)

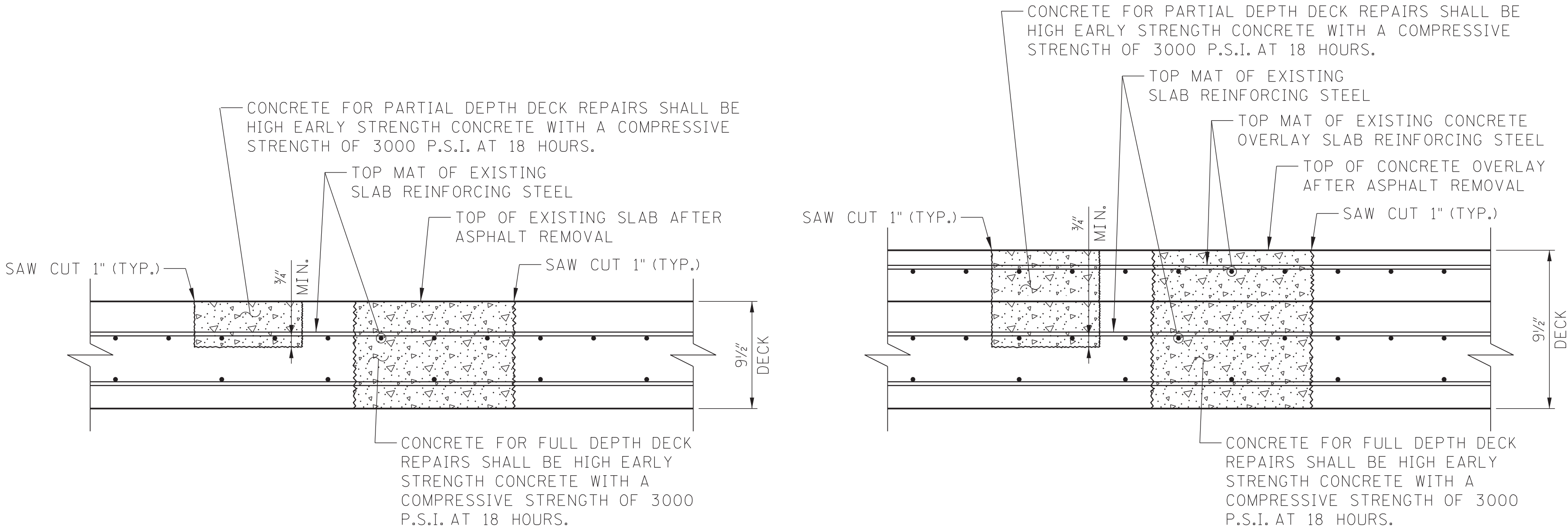
IN ADDITION TO SURFACES SHOWN, ALL EXPOSED CONCRETE SURFACES ON THE BENTS AND ABUTMENTS ARE TO RECEIVE AN APPLIED TEXTURE FINISH, MOUNTAIN GRAY, FED. SPEC. NO. 36440.

COST OF TEXTURE COATING SHALL BE INCLUDED IN ITEM NO. 604-04.02.

THE CONTRACTOR SHALL USE CONTAINMENT SCREENS OR OTHER MEASURES AS NECESSARY TO PREVENT ANY TEXTURE COATING FROM ENTERING THE ENVIRONMENT. CONTAINMENT MEASURES SHALL BE APPROVED BY THE ENGINEER AND COST SHALL BE INCLUDED IN ITEMS BID ON.

THE EXISTING SURFACES THAT ARE TO RECEIVE A TEXTURE FINISH SHALL BE FREE OF ALL FLAKING TEXTURE COATING, RUST, DIRT, OIL, AND OTHER FOREIGN SUBSTANCES PRIOR TO THE APPLICATION OF THE TEXTURE FINISH. THE SURFACE SHALL BE CLEANED TO THE COMPLETE SATISFACTION OF THE ENGINEER USING A HIGH PRESSURE WATER WASH. COST TO BE INCLUDED IN ITEM NO. 604-04.02.

THE WASH WATER IS TO BE FILTERED AND PAINT CHIPS AND DEBRIS COLLECTED PRIOR TO RELEASE OF WATER.



DETAIL SHOWING FULL AND PARTIAL DEPTH DECK REPAIR

REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF 3/4" BELOW THE TOP BAR OF THE TOP MAT OF REINFORCING STEEL. ALL REINFORCING STEEL IN AREAS OF DECK REPAIR SHALL BE COMPLETELY CLEANED. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM HEADQUARTERS, BRIDGE INSPECTION AND REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST THREE (3) DAYS IN ADVANCE. DECK REPAIR WILL BE PAID FOR UNDER ITEM NO. 604-10.50, BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB), ITEM NO. 604-10.30, BRIDGE DECK REPAIR (FULL DEPTH OF SLAB), ITEM NO. 604-10.56, CONCRETE OVERLAYED BRIDGE DECK REPAIR (PARTIAL DEPTH), AND ITEM NO. 604-10.34, CONCRETE OVERLAYED BRIDGE DECK REPAIR (FULL DEPTH). DURING PARTIAL DEPTH REPAIRS, SHOULD DETERIORATED CONCRETE BE ENCOUNTERED WHICH APPEARS TO RUN FULL DEPTH IN THE SLAB, THE ENGINEER MAY DESIGNATE THESE AREAS TO BE REPAIRED UNDER ITEM NO. 604-10.30 OR ITEM NO. 604-10.34. POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING FULL AND PARTIAL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

1. PARTIAL DEPTH REPAIRS - PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60-POUND CLASS SHALL NOT BE USED.
2. FULL DEPTH REPAIRS - PNEUMATIC HAMMERS HEAVIER THAN A 60-POUND CLASS SHALL NOT BE USED. ALSO ALL DECK REPAIR OVER BEAMS WILL BE RESTRICTED TO 35-POUND PNEUMATIC HAMMERS.
3. CHIPPING HAMMERS OF THE 15-POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

ITEM NO. 604-10.30, 604-10.50, 604-10.34, AND 604-10.56 MAY BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS
BRIDGE NO. 71-I0040-33.46
INTERSTATE 40 (R&L) OVER S.R. 24
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2017



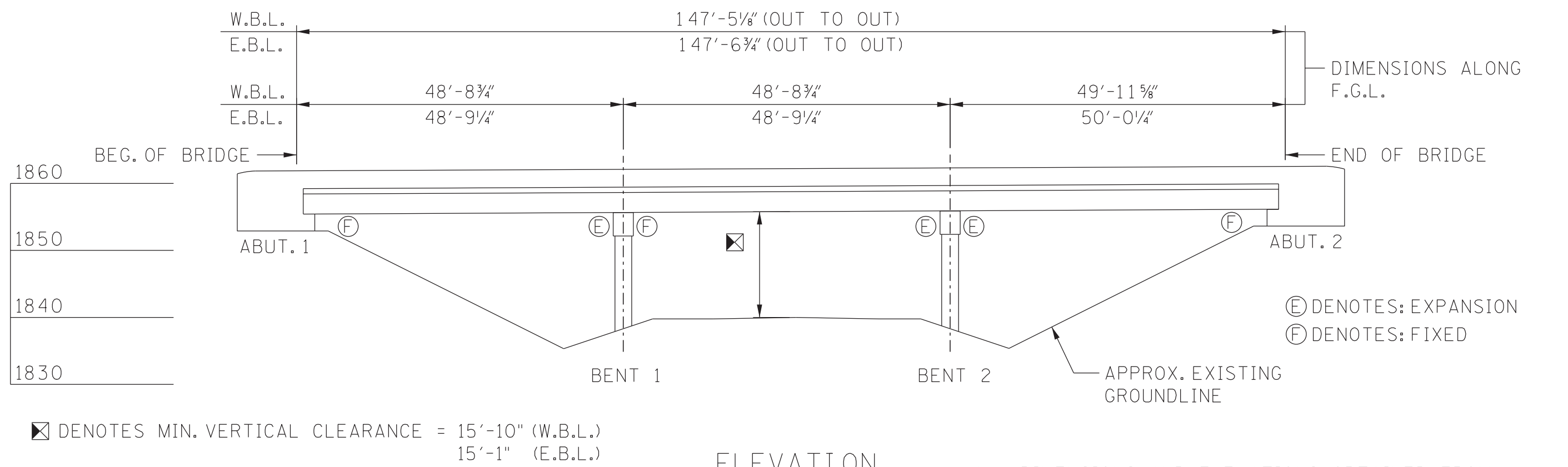
DESIGNED BY H.H. NEAL DATE 11/2016
DRAWN BY M.D. SIMPSON DATE 11/2016
SUPERVISED BY G.S. WILSON DATE 11/2016
CHECKED BY G.S. WILSON DATE 11/2016

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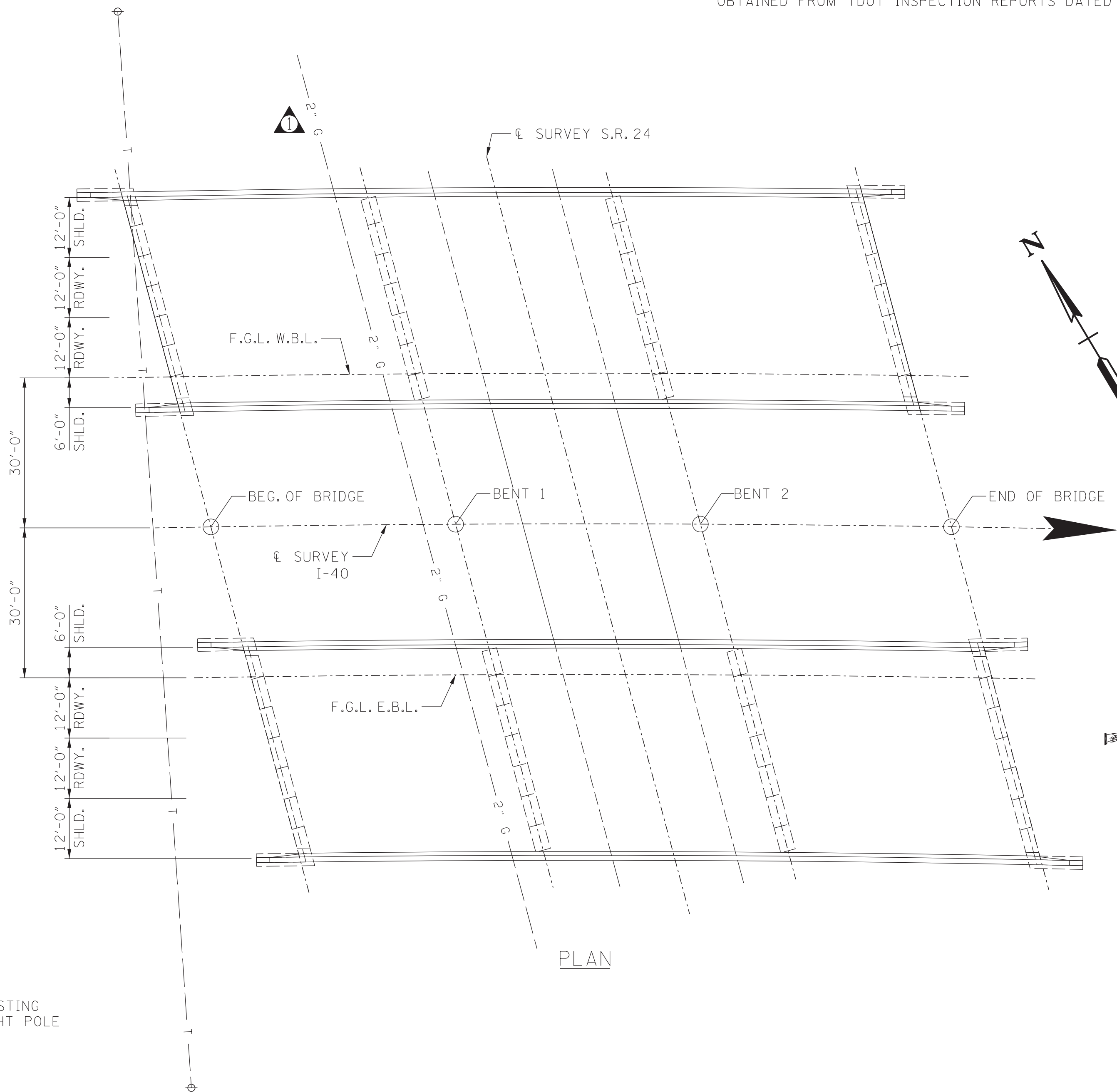


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ELEVATION

ALL DIMENSIONS AND ELEVATIONS OBTAINED FROM EXISTING PLANS DATED 1960 AND 1981. ALL CLEARANCES OBTAINED FROM TDOT INSPECTION REPORTS DATED 2014.



PLAN

SEE ORIGINAL PLANS FOR VERTICAL AND HORIZONTAL CURVE DATA



SCOPE OF WORK

1. PROVIDE REQUIRED TRAFFIC CONTROL AND PHASED CONSTRUCTION MAINTAINING ONE LANE OF TRAFFIC ON EACH BRIDGE.
2. PROVIDE TRAFFIC CONTROL FOR SR 24 (BELOW).
3. REMOVE EXISTING ASPHALT OVERLAY ON BRIDGES.
4. RECONSTRUCT THE CONCRETE BRIDGE DECKS IN AREAS OF FULL AND PARTIAL DEPTH DECK REPAIR (UTILIZE 18 HOUR CONCRETE).
5. ADD REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS (PABE'S) TO EACH END OF EACH BRIDGE (UTILIZE HIGH EARLY STRENGTH, 10 DAY CONCRETE).
6. PLACE NEW 3 1/4" ASPHALT SEAL WITH MEMBRANE APPLIED DIRECTLY ON BRIDGE DECK AND CONCRETE PABE'S. ASPHALT SHALL CONSIST OF B-MODIFIED AND GRADING "D" MIXES. UTILIZE SNOWPLOWABLE MARKERS AS REQUIRED.
7. REPAIR DAMAGED/DETERIORATED AREAS OF CONCRETE BEAMS.
8. REPAIR DETERIORATED AREAS OF CONCRETE ON DIAPHRAGMS AND SUBSTRUCTURE UNITS.
9. REPAIR DETERIORATED AREAS OF CONCRETE PIER PROTECTION ALONG SR 24.
10. EPOXY-INJECT CRACKS IN BEAMS, DIAPHRAGMS, AND SUBSTRUCTURE UNITS.
11. CONSTRUCT ASPHALT PAVEMENT TRANSITIONS AT ENDS OF PABE.
12. HIGH PRESSURE WATER WASH AND TEXTURE COAT CONCRETE SURFACES OF THE BRIDGE RAILING, DECK OVERHANGS, BOTTOM AND OUTSIDE FACE OF FASCIA BEAMS, BOTTOM OF INTERIOR BEAMS, ALL EXPOSED SURFACES OF THE SUBSTRUCTURES, AND TRAFFIC AND TOP FACES OF PIER PROTECTION ALONG S.R. 24.
13. ALL DIRT/DEBRIS/TRASH SHALL BE REMOVED FROM SUBSTRUCTURES AND BELOW BRIDGE (COST TO BE INCLUDED IN ITEMS BID ON).
14. REMOVE VEGETATION FOR A DISTANCE OF TEN FEET ALONG EACH SIDE OF THE BRIDGES AS DIRECTED BY THE ENGINEER.
15. REPLACE BRIDGE END DRAIN (RIGHT BRIDGE, RIGHT SIDE, ABUTMENT 1).

LIST OF BRIDGE DRAWINGS

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SUBSTRUCTURE REPAIRS (BRIDGE NO. 71-10040-34.12)	BR-124-29	

LIST OF BRIDGE REFERENCE DRAWINGS

(TO BE PRINTED WITH PLANS)

M-86-104, M-86-106 THRU M-86-112,
M-28-1, K-1-781 THRU K-1-788

POSTED SPEED LIMIT (I-40) = 70 M.P.H.
ADT (2017) = 42,234

POSTED SPEED LIMIT (SR 24) = 45 M.P.H.
ADT (2017) = 9,145

F.G.L. = FINISHED GRADE LINE
E.B.L. = EASTBOUND LANE (RIGHT BRIDGE)
W.B.L. = WESTBOUND LANE (LEFT BRIDGE)

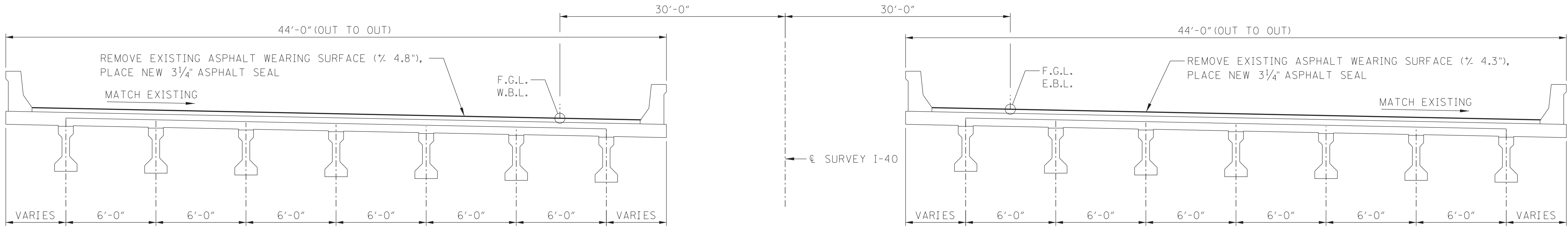
PROJECT NO.		YEAR	SHEET NO.
71100-4123-04		2017	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	01-18-17	MDS	ADDED GAS LINE, UPDATED LAST REV. DATE



STATE OF TENNESSEE
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LAYOUT OF
BRIDGE TO BE REPAIRED
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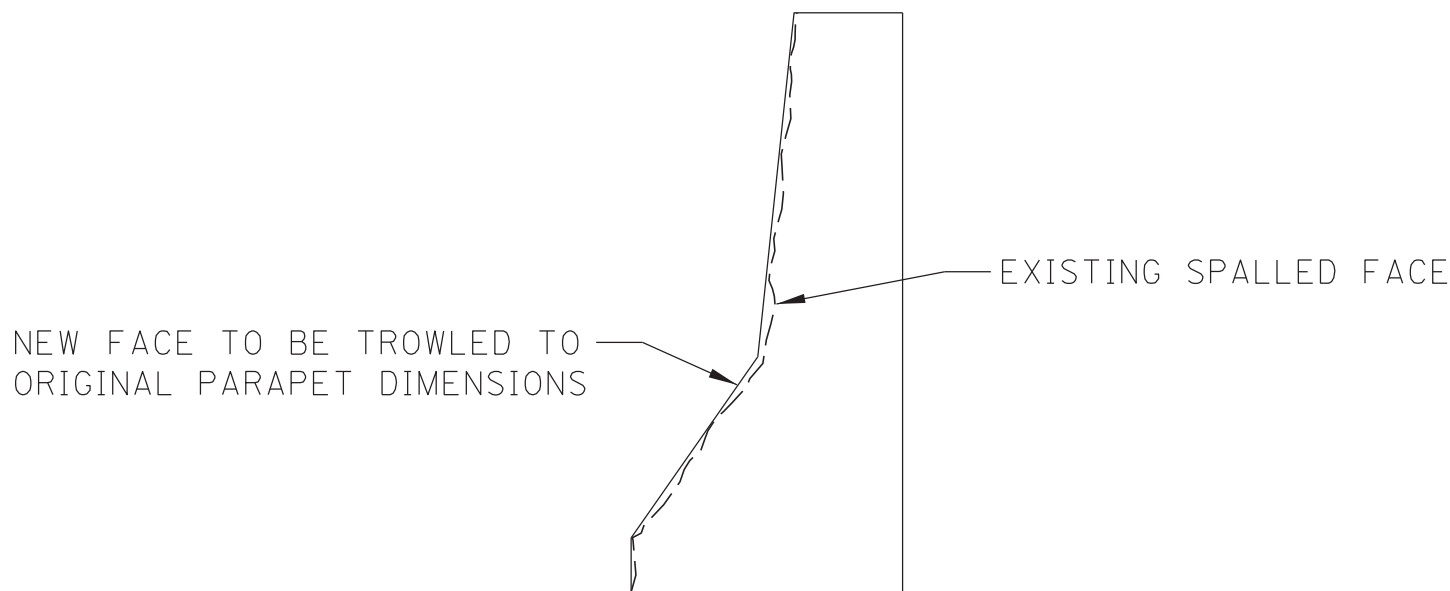


LEFT BRIDGE

TYPICAL SECTION
(LOOKING AHEAD)

RIGHT BRIDGE

COST OF ASPHALT WEARING SURFACE REMOVAL TO BE INCLUDED IN ITEM NO. 604-10.14, REMOVE EXISTING WEARING SURFACE, L.S.



APPLIED TEXTURE FINISH DETAILS

SEE SHEET BR-124-19

PIER PROTECTION REPAIR

EXTREME CARE SHALL BE TAKEN WHEN REMOVING THE EXISTING SPALLED CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED EXISTING REINFORCING STEEL SHALL RECEIVE A COMPLETE CLEANING TO REMOVE ALL RUST. ALL EXISTING REINFORCEMENT SHALL REMAIN IN PLACE. ALL WORK MUST MEET WITH THE FULL APPROVAL OF THE ENGINEER.

COST OF REMOVING SPALLED CONCRETE, CLEANING RAIL, COMPLETELY CLEANING EXPOSED REINFORCING STEEL, CONCRETE, LABOR AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN ITEM NO. 622-01. PNEUMATICALLY PLACED CONCRETE, S.F.

IF SCALED REPAIR AREA IS MORE THAN 1/4" DEEPER THAN ORIGINAL PARAPET FACE. CONTRACTOR IS TO USE SIKAREPAIR 223 WITH SIKALATEX R. FOR REPAIR AREAS SHALLOWER THAN 1/4" CONTRACTOR SHALL USE SIKATOP SEAL 107.

DETAIL SHOWING FULL AND PARTIAL DEPTH DECK REPAIR

SEE SHEET BR-124-19



STATE OF TENNESSEE
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